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Washoe County Recorder

Kathryn L. Burke - Recorder

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The Pines



Supplemental Development Standards (PSDS) Handbook

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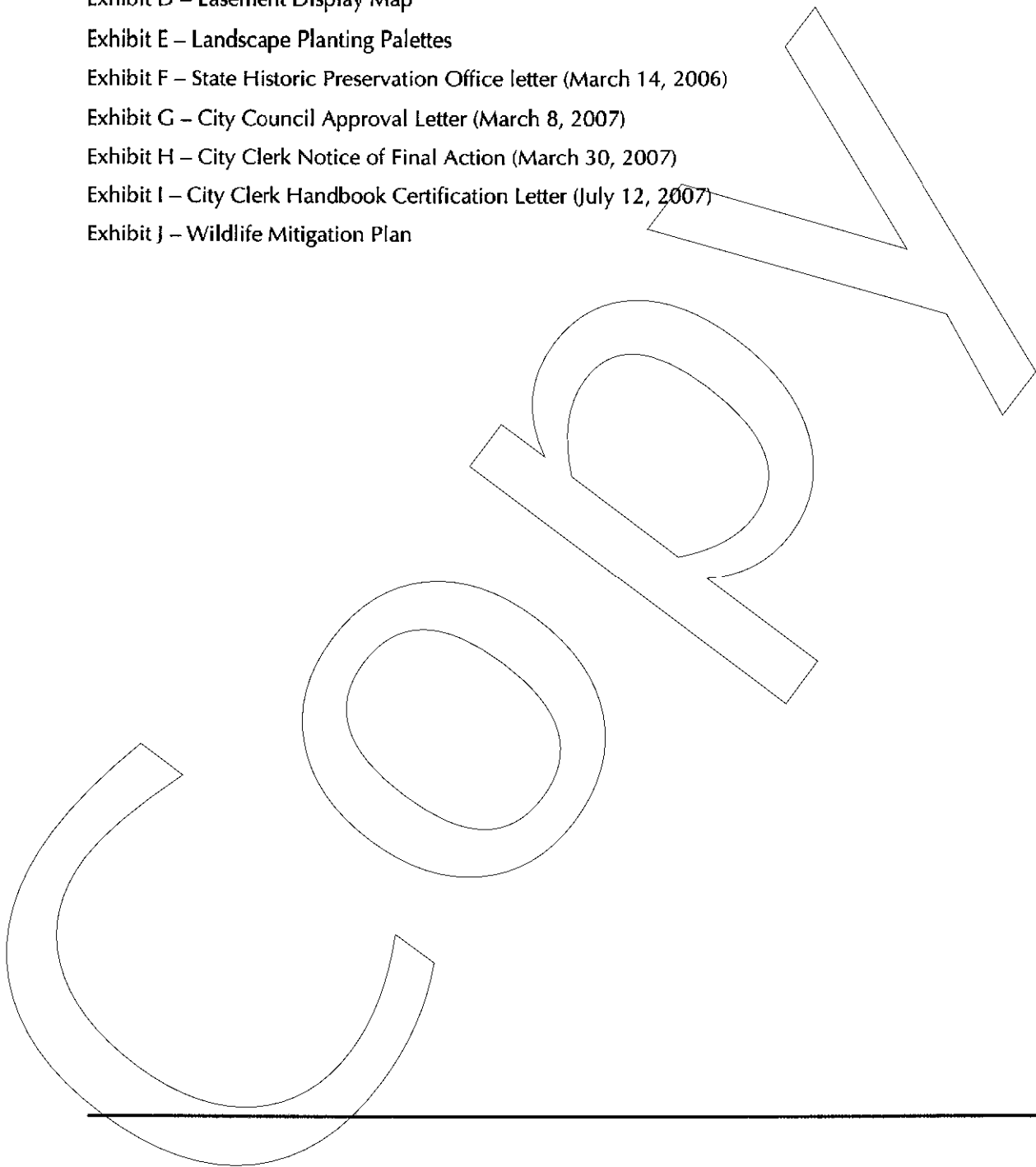
Exhibit F – State Historic Preservation Office letter (March 14, 2006)

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The Pines - Supplemental Development Standards Handbook

Objective

The purpose of The Pines Supplemental Development Standards (PSDS) handbook is to create a unique, integrated residential community that incorporates fifteen separate parcels consisting of 625 acres of land that will be included in the Caughlin Ranch Planned Unit Development (PUD). The property will be annexed into the Caughlin Ranch Covenants Codes and Restrictions (CC & R's) and shall become a part of the Homeowner's Association (HOA) upon recordation of each final map in the project. The PSDS handbook establishes the design parameters that ensure the following goals will be met:

- Quality design and development in Caughlin Ranch;
- Consistency in application of development standards between the PSDS handbook and the Caughlin Ranch PUD; and
- A framework for the City of Reno staff to evaluate future tentative and final subdivision maps, final improvement plans, grading plans, individual architectural plans for the future homes and building permit requests in The Pines.
- A framework for the Caughlin Ranch Architectural Control Committee (CRACC) to evaluate final maps (under the parameters of the City of Reno's 30 day final map review process) and individual architectural and landscaping plans with each building permit for the future homes in The Pines.

Property Location

The Pines, is generally located at the south end of Caughlin Ranch in southwest Reno. Adjacent land uses to the north include an existing mini storage/RV storage facility, the Traditions and Whispering Pines single family subdivisions of Caughlin Ranch, the Quail Valley in The Pines subdivision which is not a part of Caughlin Ranch and undeveloped property under the ownership of Washoe County. United States Forest Service (USFS) land is located to the west and south of the site. Undeveloped private property owned (as of the date of the PSDS handbook) by Sierra Land Associated, Inc. is located adjacent to the southwest boundary. The Ballardini Ranch, which is also undeveloped, is located to the east and (as of the date of the PSDS handbook) is under the private ownership of Evans Creek LLC (refer to Figure 1 - Caughlin Ranch Master Plan).

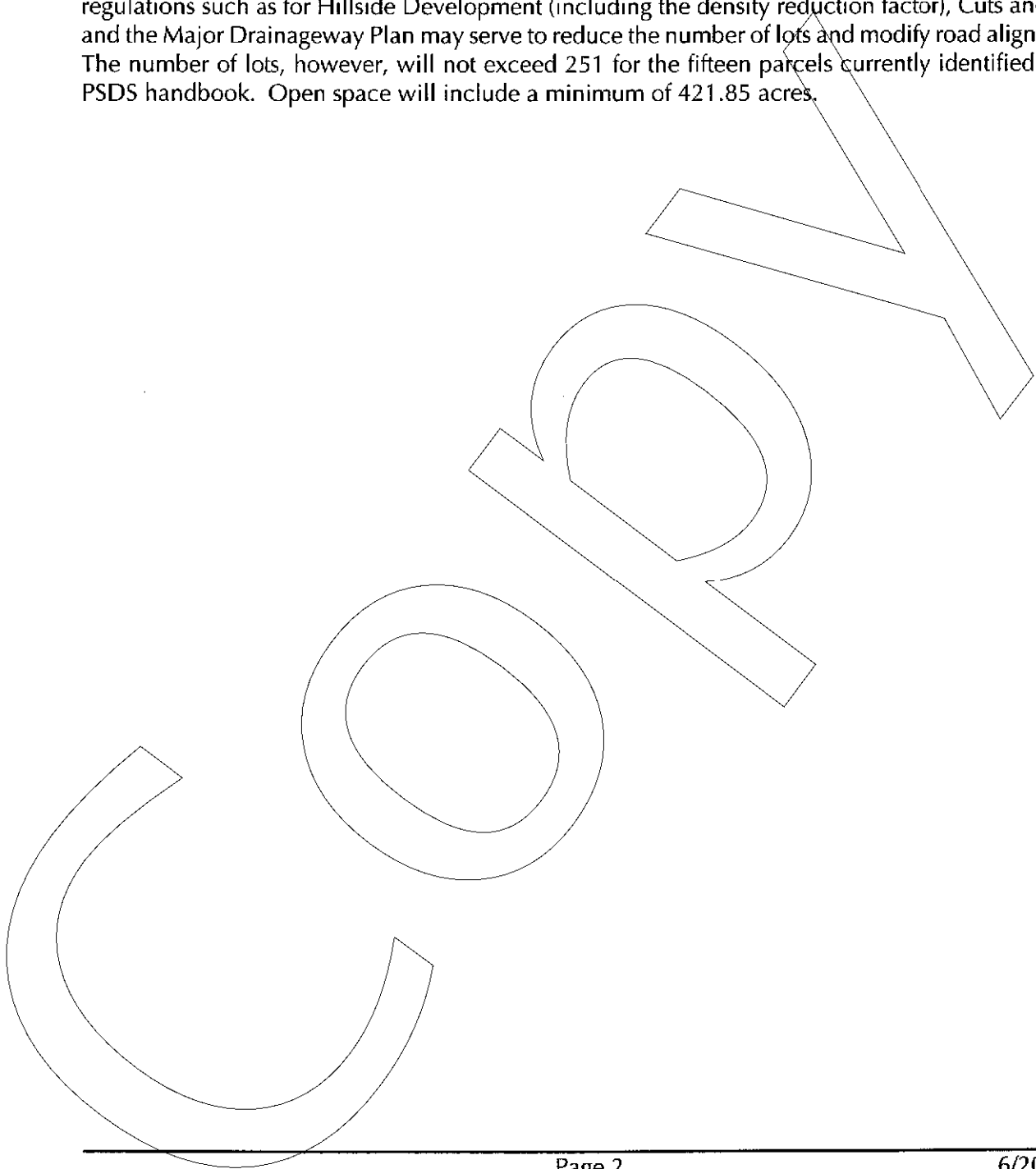
Project Concept/Description

The Pines lots are nestled on hillsides that will require careful grading, landscaping, and hillside adaptive architecture to establish custom home sites. The Pines PSDS handbook consists of a Master Development Plan for a total of 251 lots on 625+/- acres that will be incorporated into the Caughlin Ranch PUD (refer to Figure 2 - Master Development Plan). Custom and semi custom home sites will be nestled throughout the development. Hillside adaptive architecture and grading techniques will be used to minimize site disturbance and create a unique development that takes advantage of the opportunities and constraints associated with the property. Lot sizes will vary but will be a minimum of .40 acres in size. The overall density of the project is approximately .40 dwelling units per acre. To ensure the project is unique and preserves natural features, approximately 67% of the site will be

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retained in common open space. Hillside grading techniques to minimize scarring will be implemented. Also, hillside adaptive architecture will be used to provide architecture that fits with the terrain.

It is the intent to develop the property in accordance with the Master Development Plan; however, the configuration of lots and street alignments may require modifications based on a more detailed analysis at the tentative or final map stages. In addition, the application of the City of Reno Development Code regulations such as for Hillside Development (including the density reduction factor), Cuts and Fills, and the Major Drainageway Plan may serve to reduce the number of lots and modify road alignments. The number of lots, however, will not exceed 251 for the fifteen parcels currently identified in the PSDS handbook. Open space will include a minimum of 421.85 acres.



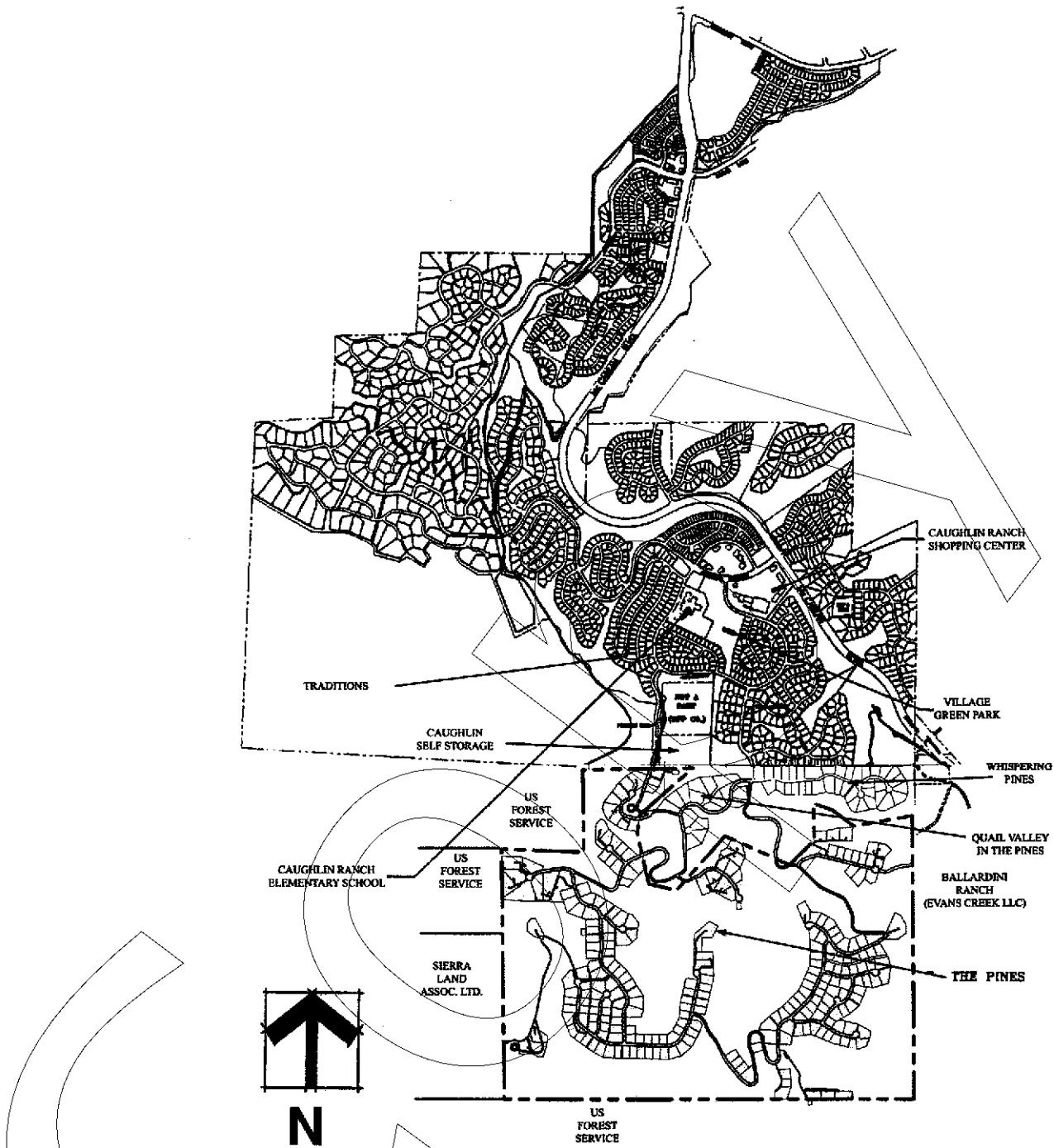


Figure 1 – Caughlin Ranch Master Plan

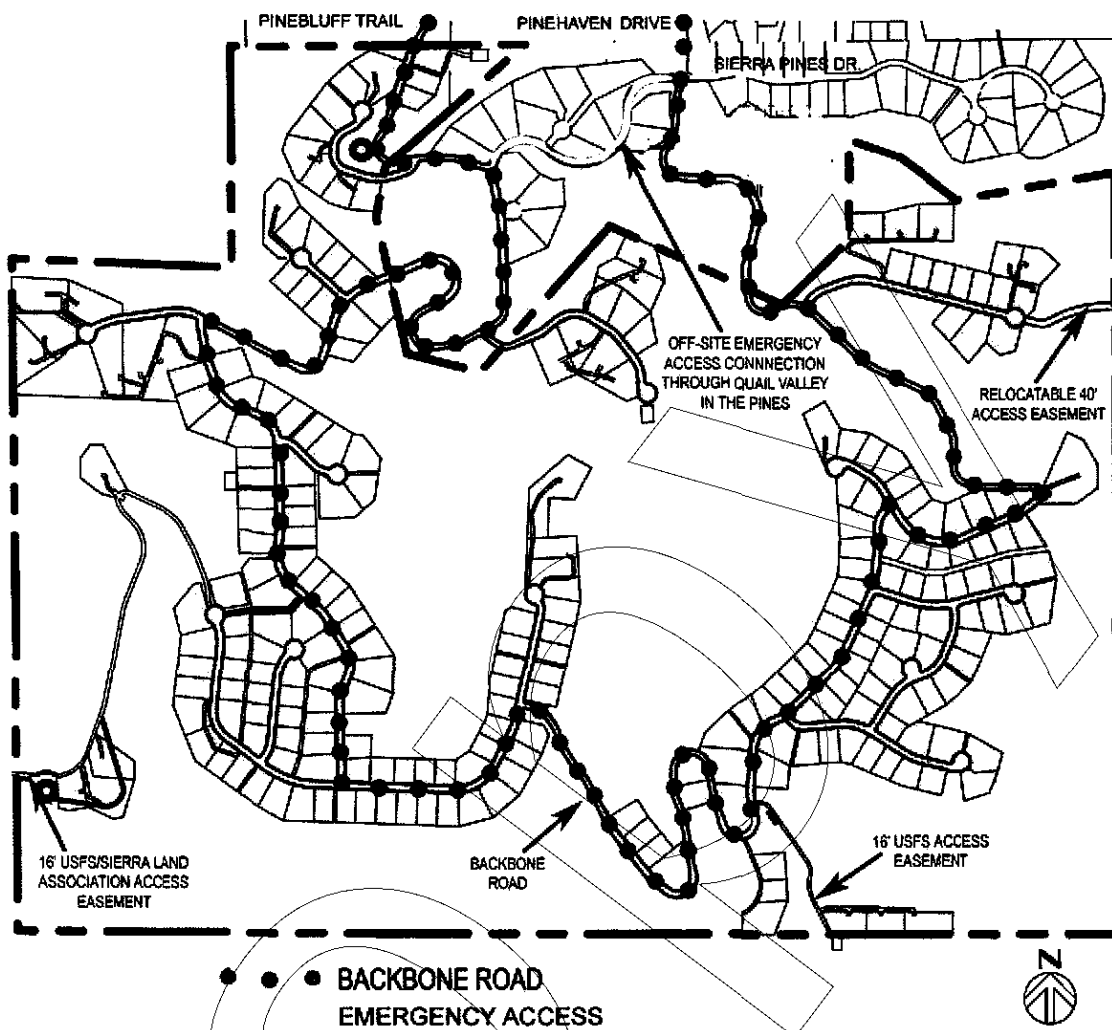


Figure 2 - Master Development Plan

Development Schedule

Build-out of the entire project is dependent on market conditions. Due to the custom and semi-custom nature of the project, build-out could span over a 20 year timeframe. The Pines however will be developed in a phased manner in accordance with the Development Schedule in the PSDS. The 20 year time frame shall start at the time of final approval (second reading of the zoning and annexation ordinances) by the City Council which occurred on March 28, 2007. If at the end of the 20 years the project is not completed, (i.e. all final maps are not recorded) then the PUD will require application for an amendment to extend the project phasing prior to the 20 year expiration date. This timeframe, however, does not apply to the construction of homes on the individual lots that have final map approval.

At the earliest, development of The Pines could begin in 2008. The project is expected to develop from north to south. A backbone road, which will be a public street, will loop through the project connecting to existing streets in Caughlin Ranch (Pinebluff Trail on the west and Pinehaven Road on the east) (refer to Figure 2 – Master Development Plan). Roadways will be constructed and paved in phases in accordance with the PSDS handbook and as approved by the Community Development Staff. Each final map submitted shall be considered as a stand alone project and shall include all public and private infrastructure for roadways, landscaping, sanitary sewer, drainage, utilities, and emergency access to meet City requirements. Construction management and access plans for traffic routes and construction hours shall be approved by the Community Development Staff and shall, to the greatest extent possible, avoid conflicts with the existing elementary school located on upper Village Green Parkway. All construction staging and construction material storage for every phase of the project shall occur on The Pines property.

In terms of future potential access to adjacent properties, The Pines will provide a 40 foot wide relocatable public roadway and access easement from the end of the northeasterly cul-de-sac in the project area to the property boundary to the east (owned as of the date of the PSDS by Evans Creek, LLC) with the associated final map (refer to figure 2 – Master Development Plan). Additionally, it should clearly be understood that Evans Creek, LLC is not a participant in The Pines PUD and is not bound by The Pines PUD to provide or connect to any access in The Pines.

The Pines master developer will provide easements and/or paved access points to the west and south property lines as shown in Figure 3 – Roadway Phasing Plan. Unimproved access to USFS existing trails and off road recreational areas on lands to the south and west will be available throughout construction unless deemed unsafe by Community Development Staff. Any new points of access required by the USFS for fire suppression only shall be granted if necessary. Improvement of access points or trails beyond the property shall be the responsibility of the United States Forest Service (USFS) or others. The Pines will also construct two trailhead parking areas on the property to facilitate access to USFS lands in accordance with the phasing plan outlined in this handbook. The trailhead areas and associated amenities will be maintained by the HOA (refer to Figure 16 - Trail Plan).

The intent for phasing of the project is outlined in this handbook but is subject to change based on market conditions, development of adjacent properties and infrastructure requirements. Three general phases are anticipated. A maximum of eight final maps are anticipated unless additional maps approved by Reno Community Development Administrator, however, there is no maximum or

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minimum number of lots required per final map. All phased improvement plan submittals shall stand alone for permitting and construction with designs and securities included for all out-of-phase infrastructure improvements required for the Phase.

There are a number of items that must be addressed prior to the submittal or recordation of the first final map or grading permit as required by the project Conditions of Approval as follows:

- A vegetation survey completed by a biologist or other qualified native plant specialist shall be conducted to determine the presence of the sensitive plant species Webber Ivesia prior to submittal of the first final map or grading permit, whichever is first. If Webber Ivesia is found within areas proposed for development and grading, a replacement and/or preservation plan shall be prepared. Any necessary mitigation shall be completed prior to the issuance of any certificate of occupancy and shall be secured with a letter of credit.
- Prior to the approval of each permit or final map, the applicant shall have plans and appropriate easements for the relocating or undergrounding of all overhead utilities traversing or encumbering with this project in accordance with City standards and RMC Section 18.12.603 "Underground Utility Services".
- Prior to the approval of each permit or final map, the applicant shall demonstrate all necessary on-site and off-site easement vacations, relocations, and grants are complete or in place. These easements include, but are not limited to: project construction, site access and cross access, roadways, utility access, maintenance access, sewer lines, surface drainage, storm drains, irrigation ditches, and utility improvements.
- Prior to the recordation of the first final map, the applicant shall demonstrate compliance with the stipulations in the Nevada State Historic Preservation Office letter dated March 14, 2006 for an archaeological inventory of the site (refer to Exhibit F in Appendix). A letter from this office shall be submitted to Community Development staff stating compliance with all requirements has been met.
- Prior to the approval of any permit (including grading) or final, the applicant shall demonstrate that an agreement exists between the developer of this project and the neighboring Quail Valley in The Pines project (LDC06-00188) for the grading, roadway, rockery walls, retaining structures, sanitary sewer, utility, and flood control improvements proposed within the boundaries of the Quail Valley in The Pines project. If, at the time of permit (including grading) or final map approval, no such agreement exists, the applicant shall demonstrate the proposed improvements within the boundaries of the Quail Valley in The Pines meet the following requirements:
 - a. Adequate easements or rights-of-way are in place for the installation, construction, ownership, access, and maintenance of all proposed improvements and appurtenances thereto (private and public) to the satisfaction of the City Engineer. These improvements and activities include, but are not limited to: grading, roadways, rockery walls, retaining structures, sanitary sewer, storm drains and culverts, public utilities, landscaping, erosion control and revegetation measures, SWPPP mitigations and the

extents of temporary and permanent earthwork cut/fill embankments.

- b. All proposed improvements and appurtenances shall be in conformance with The Pines Supplemental Development Standards.
 - c. Adequate provisions for access shall be provided to all improvements and appurtenances thereto (private and public) in accordance with the Public Works Design Manual. All roadway and sanitary sewer improvements within the Quail Valley in The Pines boundaries shall be public and maintained by the City – all other improvements and appurtenances shall be maintained by The Pines Home Owners Association (or equivalent).
- Prior to recordation of the first final map, the applicant shall provide an aerial photograph of the subject parcels to planning staff in order to locate all debris on site for removal and to identify areas of disturbance to be scarified. Debris removal and scarification may occur with each final map. A letter from the Caughlin Ranch Architectural Control Committee shall be submitted that states compliance with this condition.

Phase I.

The first general phase of the project will include:

- On-site and off-site roadway dedications, roadway, sanitary sewer, drainage, and utility extensions and improvements to the first section of the backbone loop road and the off-site connection from Pinebluff Trail to Pinehaven Road through Quail Valley in The Pines. This initial phase will specifically involve the extension of Pinebluff Trail from its current terminus in Caughlin Ranch with an emergency access connection to Pinehaven Road at its current terminus in the Quail Valley in The Pines subdivision, unless this connection or a part of the connection has already been constructed as a temporary or permanent roadway by the developer of the Quail Valley in The Pines project (refer to Figure 3 – Roadway Phasing and Figure 9a – Temporary Backbone (Emergency) Access Street Section).
- Associated temporary and permanent slope treatment and revegetation, as well as permanent landscaping and drainageway crossings along the backbone roadway/Pinebluff extension where no lots are planned within The Pines site will be the responsibility of the master developer of The Pines and will be in accordance with the PSDS handbook.
- Associated temporary and permanent slope treatment and revegetation, landscaping, drainageway crossings along the backbone roadway including maintenance where no lots are planned within the Quail Valley in The Pines Planned Unit Development will be the responsibility of the master developer of The Pines or in conjunction with the master developer of the Quail Valley in The Pines project per a separate agreement between the property owners which may include maintenance agreements based upon fair share development criteria. The City of Reno has no responsibility in negotiating separate agreements nor will the City be a party to any agreement.
- All improvements related to The Pines project associated with the first general phase of the project will either be constructed (and bonded for in accordance with City of

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Reno requirements) by the master developer of The Pines, or in conjunction with the developer of the Quail Valley in The Pines project per a separate agreement between the property owners which may include maintenance agreements based upon "fair share" development criteria. The City of Reno has no responsibility in negotiating separate agreements nor will the City be a party to any agreement.

- The first phase final map will likely involve the development of lots 1-11 (as shown on the tentative map in Appendix) near the northwest property boundary that can be served with roads in compliance with Reno Fire Department requirements for this project (i.e. cul-de-sacs and roads that generally do not exceed 1,200 feet in length and/or roads that include a secondary access connection or easement such as lots 237 – 248 as shown on the tentative map in the Appendix).
- This phase will also include construction of the Caughlin #5 water tank (Tank 1) and the associated booster pump station by the Truckee Meadows Water Authority (TMWA). Water system improvements including Tank 1, Booster Station 1, associated access roads, permanent slope treatment and permanent landscaping will be constructed and installed by TMWA per the PSDS.

Phase II.

The second general phase of the project will involve the following:

- Dedication of full width public right-of-way, extension of sanitary sewer, storm drainage facilities, drainageway crossings, utilities and construction of emergency access roadway surface improvements as shown in Figure 3-Roadway Phasing Plan and Figure 9a – Temporary Backbone (Emergency) Access Street Section for the entire backbone loop road.
- Associated temporary and permanent slope treatment and revegetation, landscaping and drainageway crossings along the backbone roadway/Pinebluff extension where no lots are planned within The Pines site will be the responsibility of the master developer of The Pines and will be in accordance with the PSDS handbook.
- Associated temporary and permanent slope treatment and revegetation, as well as permanent landscaping, drainageway crossings along the backbone roadway including maintenance where no lots are planned within the Quail Valley in The Pines Planned Unit Development will be the responsibility of the master developer of The Pines or in conjunction with the master developer of the Quail Valley in The Pines project per a separate agreement between the property owners which may include maintenance agreements based upon fair share development criteria. The City of Reno has no responsibility in negotiating separate agreements nor will the City be a party to any agreement.
- All improvements related to The Pines project associated with the second general phase of the project will either be constructed (and bonded for in accordance with City of Reno requirements) by the master developer of The Pines or in conjunction with the developer of the Quail Valley in The Pines project per a separate agreement between the property owners which may include maintenance agreements based upon "fair share" development criteria. The City of Reno has no responsibility in negotiating separate agreements nor will the City be a party to any agreement.
- The construction of the entire backbone loop roadway with emergency access roadway surface improvements as shown in Figure 9a - Temporary Backbone

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(Emergency) Access Street Section will meet secondary access requirements for the project and will allow for final maps to be submitted for development of lots on the east and west side of property.

- The backbone loop road will be paved and permanently landscaped and sanitary sewer, storm drain facilities, and utilities will be extended, where required, incrementally on The Pines property to serve development of lots as final maps are approved and as the project progresses from north to south, as approved by the Community Development Staff.

Phase III.

Following completion of the temporary backbone loop connection, the third general phase will include the following:

- Subsequent final maps within the project area will be filed with associated paving and permanent landscaping, as well as other related improvements provided.
- Subsequent water system improvements including remaining booster stations and Tank 2 as required by TMWA will be constructed. Associated access roads, temporary and permanent slope treatment (revegetation only) will be provided per the PSDS.
- Easements and access points to USFS land to the south and west as shown on Figure 16 – Trail Plan will be graded and sanitary sewer, storm drain facilities and utilities will be extended where required.
- Associated temporary and permanent slope treatment and revegetation, per the PSDS handbook will be installed.
- The southerly access point to USFS property will be paved to the project boundary when the road is paved to serve the lots adjacent to the USFS access road as shown on Figure 2 - Master Development Plan.
- An easement for the westerly access point will be granted and the road will be paved (and sanitary sewer, storm drain facilities, and utilities will be extended, where required) with the completion of the second water tank (Tank 2).

Overall off-site roadway and transportation improvements and phasing will be provided as follows:

- The master developer of The Pines shall have approved plans for roadway striping improvements along Village Green Parkway (west), from Caughlin Parkway to Foxcreek Trail (south). These improvements shall include a double solid yellow centerline and single solid white shoulder lines (each side) to create 12-foot wide travel lanes. These improvements shall be installed by the master developer of The Pines and shall be complete and functional prior to the issuance of any certificate of occupancy.
- Prior to issuance of any on site permit, including grading and subsequently again prior to the issuance of the certificate of occupancy for the 143rd unit, the master developer of The Pines shall submit a Traffic Calming Petition to the Public Works Engineering Department to address speeding along Village Green Parkway (east and west) and other mitigations measures (that are not already committed to be provided by the master developer) that can be addressed through the City of Reno's Traffic Calming Program process. The master developer shall make any improvements identified and approved through the initial and subsequent Traffic Calming Program processes in an amount not the exceed \$40,000 (aggregate).
- The master developer of The Pines shall have approved plans for a new crosswalk at

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the Caughlin Parkway/Village Green Parkway (east) intersection. This crosswalk shall be located on the south side of the intersection across Village Green Parkway in a manner consistent with City Standards. This improvement shall be installed by the master developer of The Pines and shall be complete and functional prior to the issuance of any certificate of occupancy.

- The master developer of The Pines shall have approved plans for a new school sign (S4-5 with 15 MPH designation) with flashers along the east side of the Village Green Parkway immediately north of the Foxcreek Trail (south) intersection. The sign shall be located to provide advanced warning of the approaching school speed zone to north bound traffic on Village Green Parkway. This improvement shall be installed by the master developer of The Pines and be complete and functional, prior to the issuance of any on-site permit, including grading.
- The master developer of The Pines shall have approved plans for roadway improvements within the Caughlin Parkway east bound approach to the Caughlin Parkway/McCarran Boulevard/Cashill Boulevard intersection. These improvements shall include roadway lane striping, surface improvements (median reconstruction and asphalt replacement), traffic signal modifications, and signage as needed to achieve dual left-turn lanes for the east bound left turn movements from Caughlin Parkway. With these improvements, the existing east bound through and right-turn lanes shall be perpetuated. All improvements and modifications shall be in accordance to the project traffic study submitted with the tentative map (and required updates thereto) and to the approval of the Community Development Department and Nevada Department of Transportation (NDOT). These improvements shall be installed by the master developer of The Pines and shall be complete and functional prior to the issuance of any certificate of occupancy. The existing median opening and turning movements at the Caughlin Parkway/Shopping Center Driveway (east) intersection shall remain with these improvements.
- Prior to issuance of the first certificate of occupancy, the master developer of The Pines shall provide all construction documents (improvement plans and reports) for required roadway improvements within the McCarran Boulevard north bound and south bound approaches to the Caughlin Parkway/McCarran Boulevard/Cashill Boulevard intersection. These improvements shall include roadway lane striping, roadway widening, surface improvements, traffic signal modifications, and signage as needed to achieve dual left-turn lanes for the north bound left turn movements and an exclusive right-turn lane for south bound traffic. All construction documents shall be in accordance with the project traffic study (and required updates thereto) and to the approval of the Community Development Department, RTC, and NDOT. These improvements shall be constructed, complete and functional, prior to the issuance of the 67th certificate of occupancy for the project.
- All construction traffic for all phases shall use the westerly route of Village Green Parkway to access Caughlin Parkway while Caughlin Ranch Elementary School is in session (mid August though mid June).

Overall trail and amenity phasing will be provided as follows:

- Prior to the issuance of a certificate of occupancy for the 25th lot, Trailhead A as shown on Figure 16 - Trail Map and the associated improvements (except paving) outlined in

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the PSDS handbook will be constructed. Paving of the parking lot will occur when the adjacent portion of the backbone road is paved.

- Prior to the issuance of a certificate of occupancy for the 50th lot, the two trails along the major drainageways identified as Trails 1 and 2 and the equestrian trail identified as Trail 4 on Figure 16 - Trail Map will be constructed.
- Trailhead B and the associated improvements including paving and Trails 1a, 2a, and 3 as identified on Figure 16 - Trail Map will be constructed prior to issuance of a certificate of occupancy for the 75th lot.

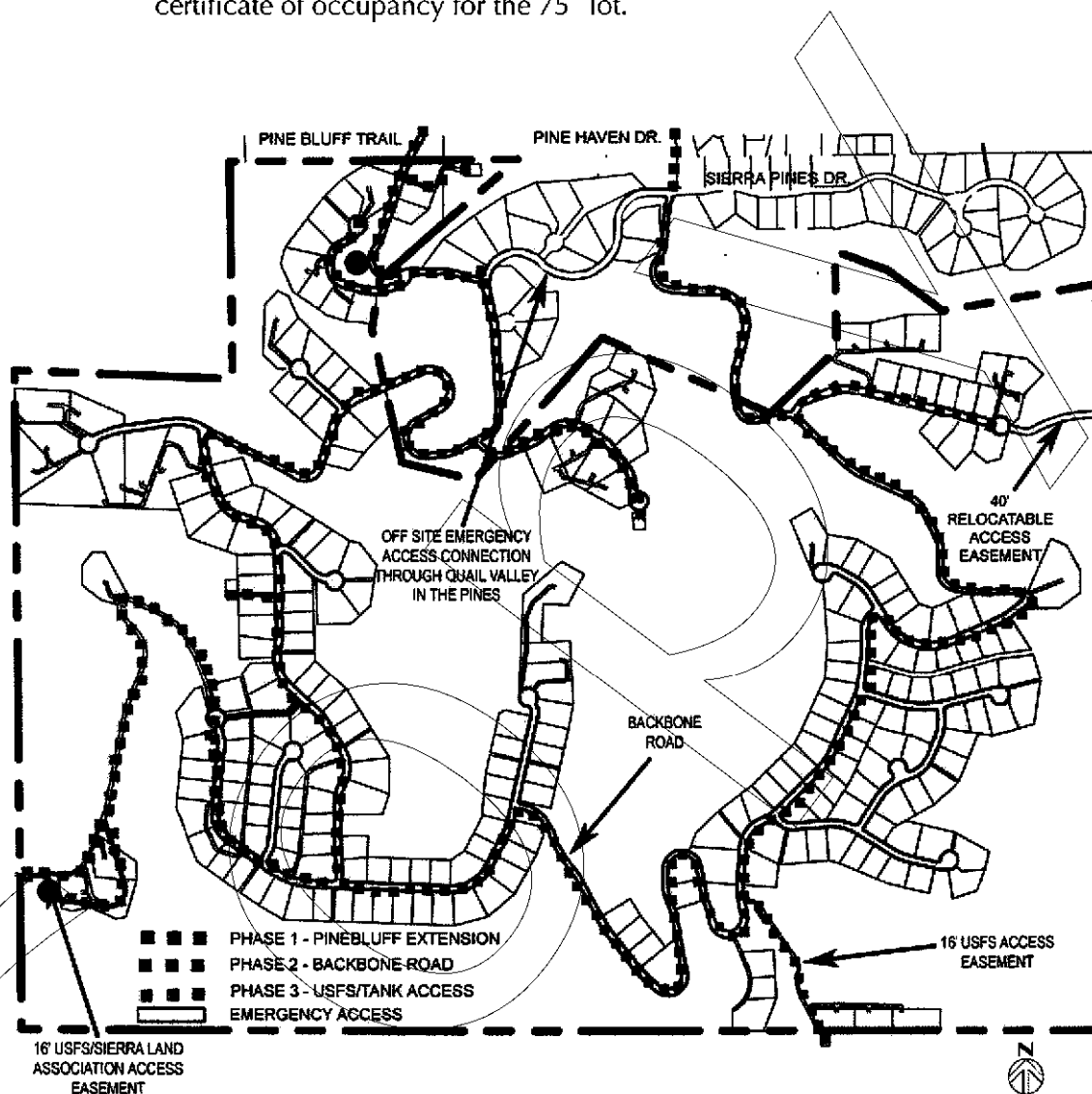


Figure 3 – Roadway Phasing Plan

Master Development Plan

The Pines Master Development Plan (refer to Figure 2 - Master Development Plan) is the product of evaluating the opportunities and constraints associated with the site. Although the project consists of fifteen separate parcels with separate ownerships, the project was designed as an integrated development. The overall project design considered slopes, reduction formulas and open space requirements in accordance with the current City of Reno Hillside Ordinance, access points to adjacent properties, street grade and emergency access requirements, overhead power lines, major drainageways, natural vegetation and views. A final slope map that shows the location of the lots, the limits of the drainageways and their fifteen foot setback and final slope calculations that indicate the amount of open space is being met per the requirements of the Hillside Ordinance in effect at the time of this project approval is included in the Appendix as Exhibit B, Final Slope Analysis Map and Exhibit C Drainageway Grade Break Setback map. A summary of the current ownership (as of the date of publication of the PSDS handbook) of each of the fifteen parcels in the development is provided in Table 1, Pines PUD Ownership. As an overall project, a total of 251 lots are planned with 421.85 acres of open space provided. As previously mentioned, it is the intent to develop the property in accordance with the Master Development Plan, however, the configuration of lots and streets may require modifications based on a more detailed analysis at the tentative or final map stages. Due to potential modifications and many design components, the handbook provides a level of detail that can only be complied with at the final design stage. All final maps must demonstrate compliance with the standards in the handbook if not adequately demonstrated to be in substantial conformance on the tentative maps. In no case shall the number of lots exceed 251. A minimum of 421.85 acres of open space shall be provided.

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Table 1 - Pines PUD Ownership Summary

Assessor's Parcel # (APN)	Owner	Parcel Size (acres)
041-230-01	Aslanian	40.18
222-161-31	Cole/Whispering Pines	13.56
041-230-05	Turner	40.92
041-230-06	BCD&S Investments	40.22
041-230-07	Turner	40.33
041-230-08	Uccelli	45.74
041-230-09	Debard-Lieberstein	40.49
041-230-10	Anastassatos	40.81
041-230-11	McCool	40.54
041-230-12	Fuller Trust	41.40
041-230-13	Thomas/Stipher	41.34
041-230-14	Rutter	42.08
041-230-15	McCool	40.09
041-021-19	Zermani-Weber	39.00
041-021-23	Benedetti	78.48
Totals		625.19

Infrastructure - Development & Phasing

Public Infrastructure

Public infrastructure will include all sanitary sewer, storm drain facilities and utilities that are typically part of a residential development project with sufficient facilities and line capacities for water, gas, electric, cable television, and telephone services. These improvements, as well as phasing, will be addressed, with the submittal of tentative and final maps. The type of roadway standards to be constructed shall be in accordance with the sections outlined in the PSDS handbook. Pedestrian trails that are shown on Figure 16, "Trail Plan" shall be shown on the tentative map. Gates restricting access to any of the public streets in The Pines or any public access routes identified on the tentative map are strictly forbidden within The Pines project. Gates, however, may be used to restrict access to individual lots or along private driveways and at water tank sites within the project area, if desired by the Truckee Meadows Water Authority (TMWA).

Full width public right-of-way dedications, emergency access roadway surface improvements and permanent improvements shall be constructed in phases for the entire backbone loop road in accordance with the development schedule outlined in the PSDS handbook. Bonding for public improvements, temporary and permanent slope treatment, revegetation, as well as permanent landscaping as required by the PSDS handbook shall be provided in accordance with City of Reno requirements.

Roadway Easements

There are existing recorded roadway easements within this project and off site that provide access to the fifteen parcels in The Pines development. The road alignments shown on Figure 2 - Master Development Plan however, do not necessarily match or "line-up" with the actual recorded easement locations as shown on Exhibit D Easement Display Map in the Appendix. The fifteen parcel owners in the PUD will therefore be required to adjust the alignment of the existing easements within the project site and through adjacent properties, as necessary with the approval of each applicable final map. In addition, the disposition of existing easements and creation of new access easements and/or right of way dedications, as appropriate, will be required prior to approval of each related final map and as determined by the Community Development Staff. In the event that off-site easements cannot be adjusted, roadways may have to be redesigned to coincide with the actual easement location.

In terms of future potential access to adjacent properties, The Pines will provide a 40 foot wide relocatable public roadway and access easement from the end of the northeasterly cul-de-sac in the project area to the property boundary to the east as generally depicted in Figure 3- Roadway Phasing Plan (owned as of the date of PSDS by Evans Creek LLC) prior to the recordation of the associated final map.

Additionally, the master developer shall have approved plans for: a) rough grading for the typical 40 foot wide street section in accordance with Figure 8A and/or 8B of the PSDS Handbook; b) an all weather emergency access roadway surface within the 40 foot wide easement/street section to the satisfaction of the Reno Fire Department; and c) an emergency access gate along this access to the satisfaction of the Reno Fire Department. Prior to the issuance of the first certificate of occupancy for

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the associated final map, the rough grading, all weather emergency access roadway surface, and the emergency access gate shall be in place, complete and functional. Should a reciprocity public roadway/access easement be in place within the parcel to the east (APN 222-080-01) and extended to this project's boundary in approximately the same location, the developer shall construct the full width improvements for a typical 40 foot wide roadway section, including roadway easement relocations as necessary, in lieu of the all weather emergency access roadway and gate to the property.

A disclosure notice shall be submitted to Community Development Staff for review and approval that alerts future residents of the most northeastern cul-de-sac and private drive on the site that the cul-de-sac could be extended for an ultimate connection to McCarran Boulevard for through traffic or emergency access. In addition, a notation shall be included in each title report for each lot on the cul-de-sac and private drive that alerts the residents of the same. Approval of the disclosure notice and title language shall be obtained prior to recordation of the affected final map.

All adjustments, new easements and dedications shall conform to the approximate alignments shown on Figure 2 - Master Development Plan for the mutual benefit of all parcels in the PUD. Alignments that serve individual parcels without regard for topography, roadway geometry, and development areas of subsequent parcels shall not be allowed.

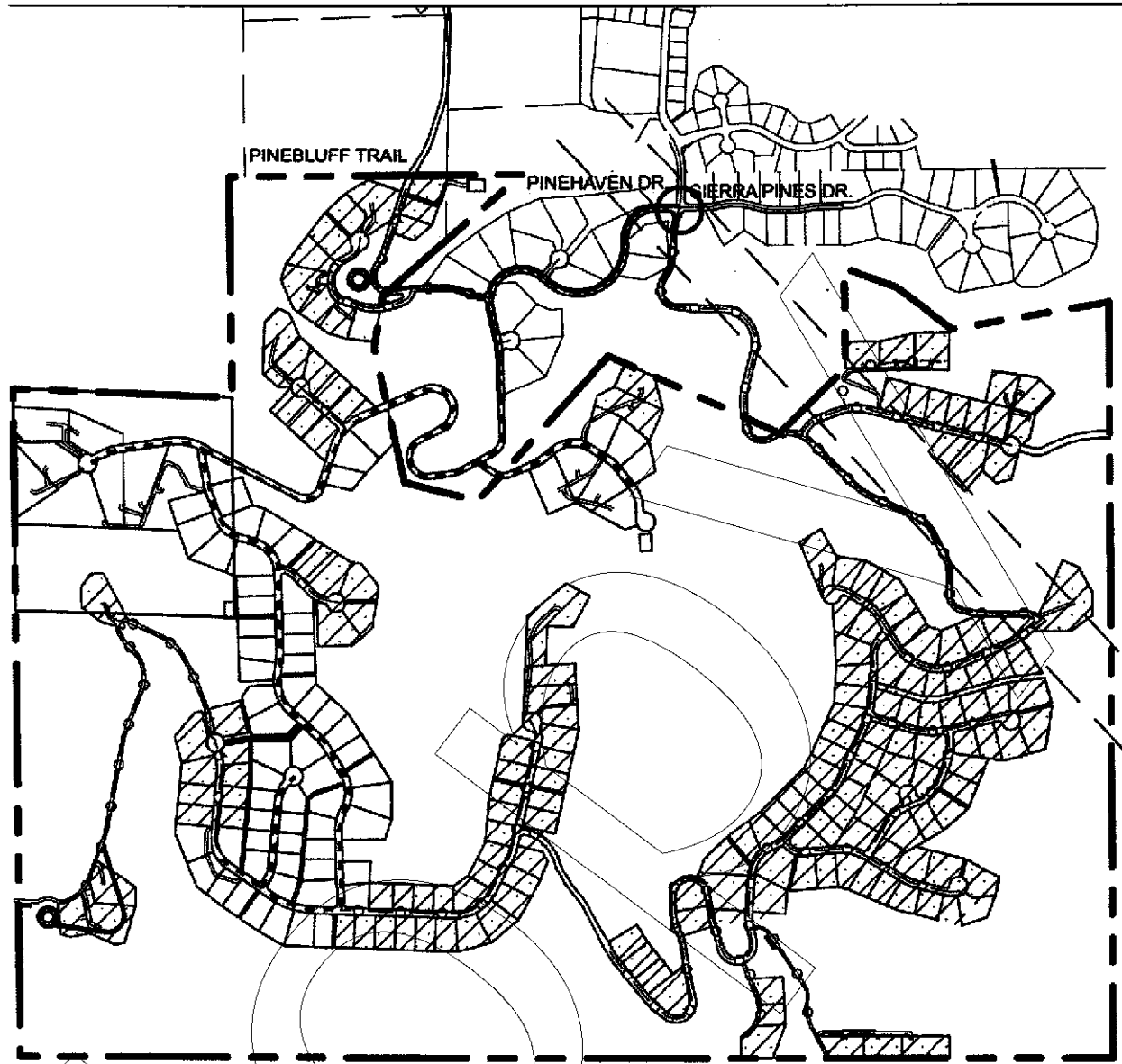
Sanitary Sewer System

There are two existing 8-inch sewer stubs at the intersection of Pinehaven Road and Sierra Pine Drive in the Caughlin Ranch area. The proposed sewer system of The Pines will connect to these existing 8-inch sewer stubs. Sewer flows from The Pines 251 lots will be added to the existing sewer system via the two existing 8-inch stubs located at the intersection of Pinehaven Road and Sierra Pine Drive.

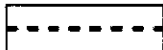
The proposed sewer system will consist of a combination of a gravity system and a low pressure system using Environment One (e-one or equivalent) grinder pumps. The low pressure system will serve 173 units and will connect to the gravity sewer system. The remaining 78 units will be served by the gravity sewer system (refer to Figure 4 - Sanitary Sewer Plan). The gravity portions of the sewer system will be offered for dedication and maintenance as public infrastructure to the City of Reno, while the low pressure system will be owned and maintained by the HOA (or equivalent).

Prior to the approval of any final map or permit, the master developer shall have a Sanitary Sewer report and Master Sewer Plan approved by the Community Development Department for the entire project. Each final map shall include provisions allowing for private access and maintenance of private sanitary sewer facilities proposed within public rights-of-way and easements. Adequate maintenance access shall be provided for all sanitary sewer improvements, public or private, per the Public Works Design Manual.

Prior to the approval of each final map or permit, improvement plans for all required off-site improvements shall be approved with securities and required rights-of-way or easements in place.



CONNECTION TO EXISTING CITY OF RENO GRAVITY SEWER SYSTEM



GRAVITY SEWER SYSTEM



E-ONE SEWER SYSTEM

** System to be built as development occurs

Figure 4 – Sanitary Sewer Plan

Water Service

The Truckee Meadows Water Authority (TMWA) has prepared a Discovery Level II report and has outlined a preliminary water facility plan to serve The Pines development. Due to the location and topography of the site, three booster pump stations and two water storage tanks are anticipated to be required to meet minimum pressure and storage needs to serve the project (refer to Figure 5 – Water Service Plan).

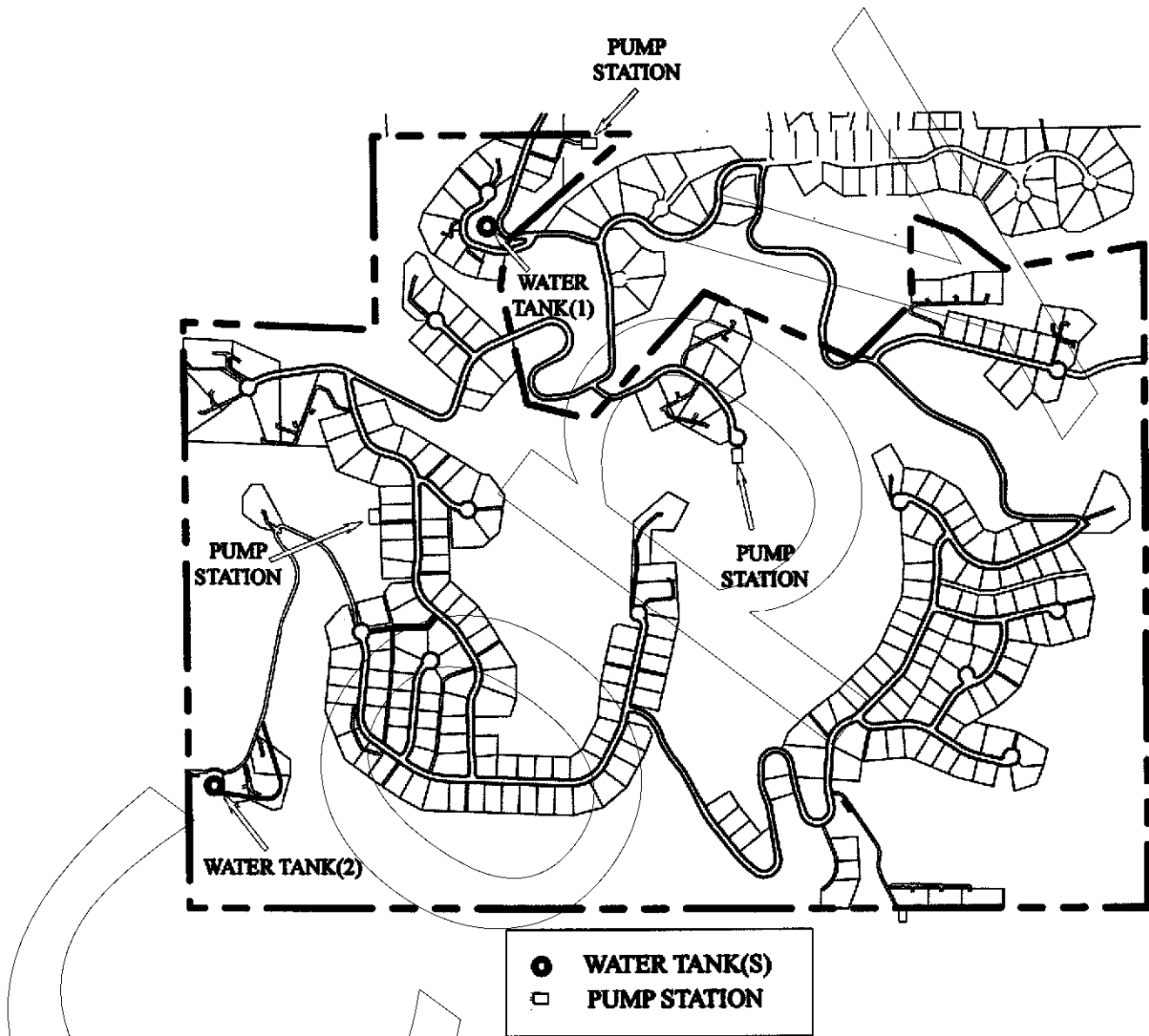


Figure 5 – Water Service Plan

As noted in the Discovery report, Tank 1 is necessary on this site to further serve the Caughlin #5 zone, which provides service to the overall Caughlin Ranch area. This tank will need to be installed prior to any new development in the Caughlin Ranch area, including development in The Pines. This tank will be located on the northwesterly portion of the site, near the entrance at Pinebluff Trail. This tank will be paid for and constructed by TMWA. In addition to the tank, a booster pump station will be located immediately south of the existing Caughlin Ranch Mini Storage facility. It is anticipated that this tank, booster pump station and subsequent upgrades to the booster pump as necessary will accommodate development of the first several lots (actual number of lots to be determined by TMWA) in the development.

Tank 2 will be constructed and installed as the project progresses by the master developer and is proposed to be located near the southwesterly property boundary. In addition to Tank 2, two additional booster stations will be required and installed per the Discovery II report provided by TMWA and when warranted by the project.

Grading and access to the tanks are subject to special use permit approval for cuts and fills by the City of Reno. Roadways providing access to the tanks shall be a minimum right of way width of 20 feet with a paved section of 16 feet. Access roads surrounding the tanks shall be per TMWA requirements. Colors of the water tanks and booster stations shall be dark brown and shall be approved by the Community Development staff. The type of landscaping and revegetative seed mix used for the tanks and booster stations shall be determined based on site conditions at the final locations for the structures as determined by TMWA. Consideration shall be given to existing trees, how close the utility is to proposed homes, and whether construction of the utility will require the elimination of existing mature trees on site. Temporary irrigation shall be provided as necessary. The CRACC and the City's Landscape Architect shall review and approve the final plans for these utilities with each associated final map.

Street Sections

The roadway network as shown in Figure 6 – Roadway Network, includes a hierarchy of roads with respect to traffic volumes and design criteria to serve The Pines development. The individual street sections outline the proper widths and grades to safely provide access to all lots, convey the expected traffic volumes, accommodate emergency vehicle access, provide access points to adjacent properties and accommodate pedestrians. The backbone loop road will be constructed and paved in phases in accordance with the PSDS handbook. Figures 7A through 8B, Street Sections, address requirements for the permanent street sections while Figure 9A addresses the requirements for the temporary emergency access street section. Gates restricting access to any of the public streets in The Pines or any identified public access routes as shown in Figure 6 are strictly forbidden within The Pines project. Gates, however, may be used to restrict access to individual lots or along private driveways and at water tank sites within the project area, if desired by the TMWA and to emergency access roads, if desired by the Reno Fire Department.

To the extent possible, streets shall be designed to follow existing topography. All street, emergency access roadways and driveway designs and their construction will be in accordance with the Public Works Design Manual to the extent possible. In areas of steep terrain, flexibility shall be considered

THE PINES

when the result will create less grading and disturbance of the native terrain, vegetation and rock outcroppings. Community Development Staff shall review and approve any changes or may require changes if deemed necessary based on final map design.

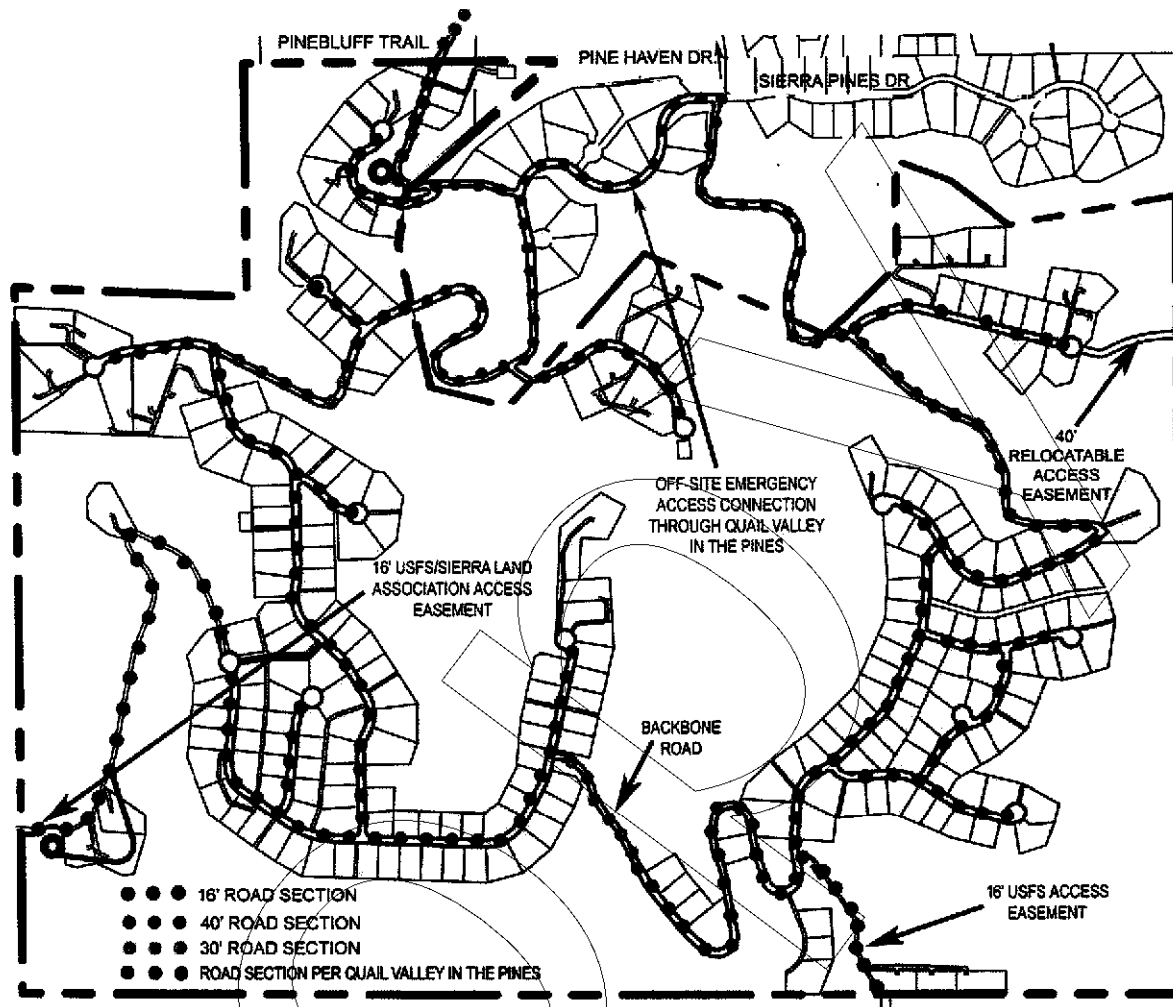


Figure 6 – Roadway Network

Street types and sections are described as follows:

Connector and Direct Access Streets

Connector and direct access streets are public streets that form the backbone loop road for the entire project and also provide direct access to the majority of the lots in the development. The backbone loop road will connect into planned streets within the Quail Valley in The Pines subdivision and existing streets within Caughlin Ranch. The westerly connection will be to Pinebluff Trail while the easterly connection will be to Pinehaven Road.

Two street sections depicting the streets at full build-out are shown in Figures 7A, 7B, 8A, and 8B. The sections include both crown and cross slope options that will be constructed and used interchangeably to best fit topographic conditions. The first section that will be constructed at build-out is intended for areas where the sole purpose is formation of the backbone loop road. This section does not provide access to any individual lots and therefore does not require on-street parking. The right of way for this street type is 30 feet wide with a paved section of 24 feet. This section includes a 4 foot wide sidewalk on one side of the street (refer to Figure 7A & 7B – 30’ Street Sections). This street section, as well as the crown and cross slope options, is in conformance with City of Reno Hillside Ordinance goals and objectives as it will help minimize grading impacts in the steeper terrain on the project site. These roads will be posted “No Parking” on both sides.

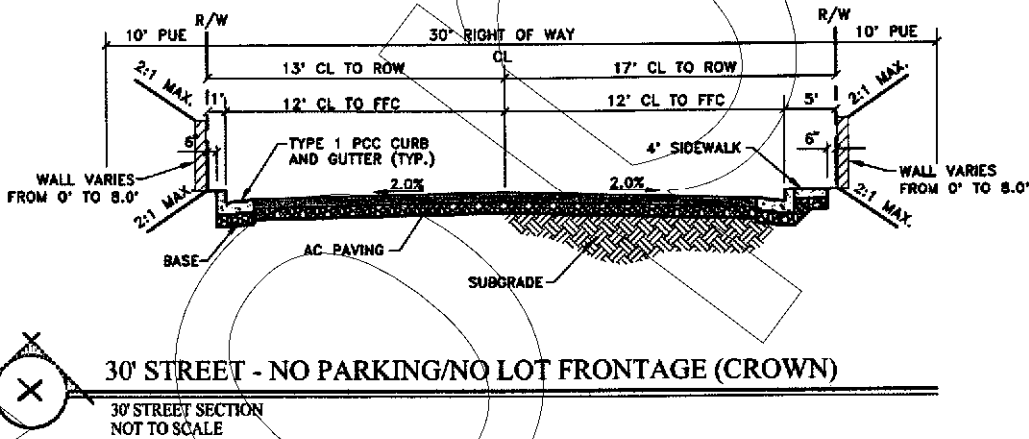


Figure 7A – 30’ Street Section

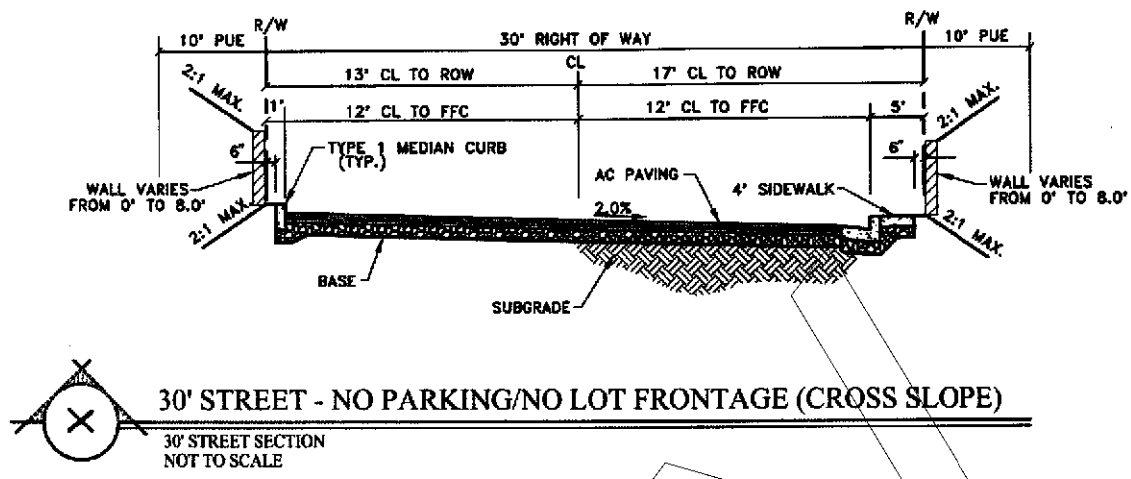


Figure 7B – 30' Street Section

The second street section to be constructed at build-out will also form the backbone loop road but will be used for direct access to lots in the development both on and off of the backbone road. Again, both crown and cross slope options are provided and will be used interchangeably to accommodate topographic conditions. The right of way for this street type is 40 feet wide with a paved section of 34 feet to allow for parking on both sides of the street in accordance with Reno Fire Department requirements. A 4 foot wide sidewalk is also provided on one side of the street. (Refer to Figure 8A & 8B – 40' Street Sections).

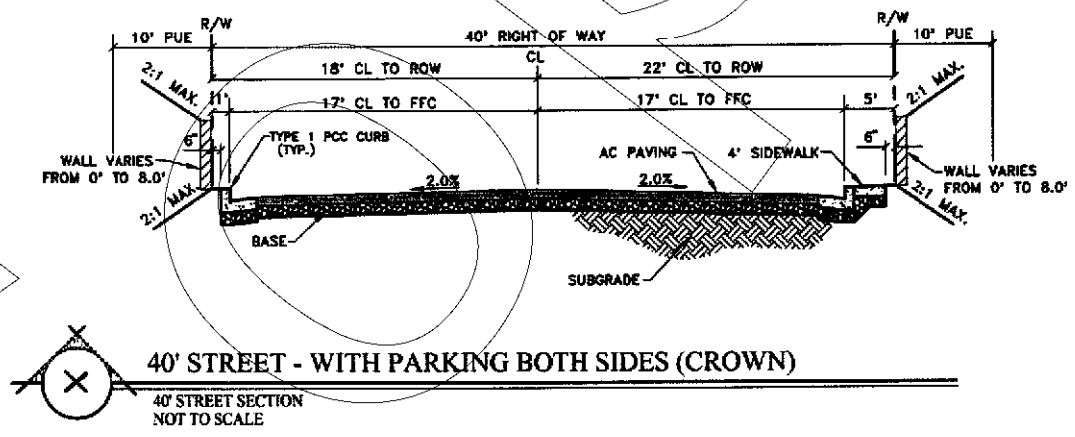


Figure 8A – 40' Street Section

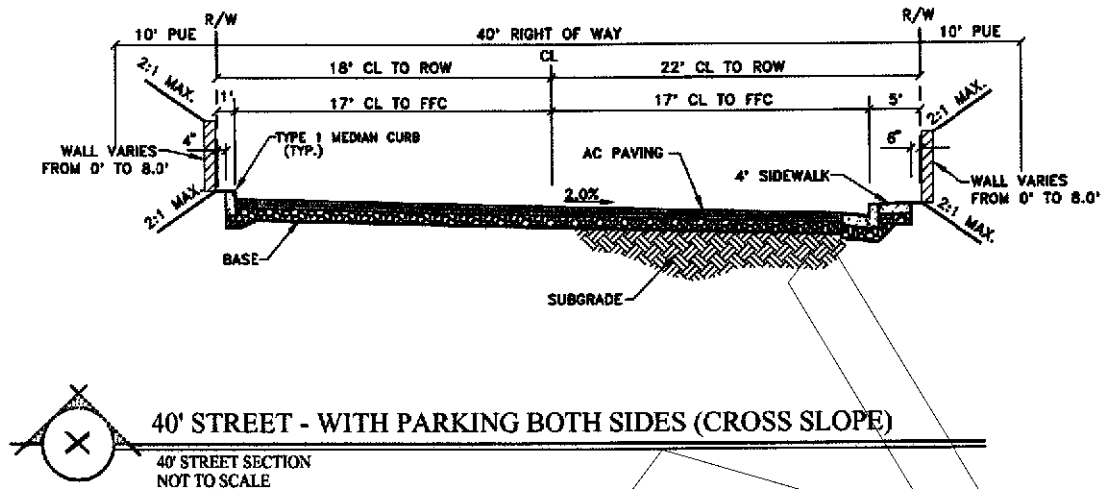


Figure 8B - 40' Street Section

Temporary Secondary (Emergency) Access Roadways

Development of The Pines will be phased from the north to the south. The project could develop in a concurrent manner from the west (Pinebluff Trail) and/or from the east (Pinehaven Road). Permanent, as well as temporary secondary access in accordance with Reno Fire Department requirements will be maintained as development progresses. Some of the lots located at the northwesterly edge of the property (lots 1 - 11) as shown on the tentative map in the Appendix will be able to develop without the construction of the entire backbone loop road. Access for these lots would be accomplished through the construction of Pinebluff Trail to its ultimate width with a connection to Pinehaven Road through the Quail Valley in The Pines development as a temporary emergency access road unless this connection or a part of the connection has already been constructed as a temporary or permanent roadway by the developer of the Quail Valley in The Pines project (refer to Figure 3 - Roadway Phasing Plan). After this connection is made and when access exceeds 1,200 linear feet the entire backbone road will need to be graded and constructed as a temporary secondary (emergency) access road. The road shall be 24 feet in width and have a gravel base in accordance with the standards outlined in the City of Reno Public Works Design Manual (refer to Figure 9A - Temporary Backbone Emergency Access Street Section). As the project progresses, full street improvements in accordance with Figures 7A & 7B - 30' Street Section and Figure 8A & 8B - 40' Street Section Standards will be provided in a phased manner as lots become ready for development.

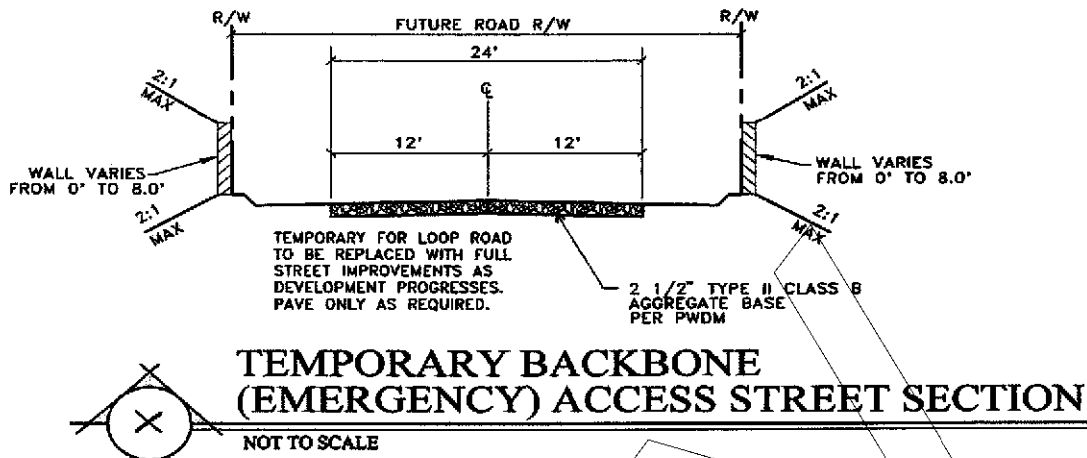


Figure 9A – Temporary Backbone (Emergency) Access Street Section

Permanent Secondary (Emergency) Access Roadways

Permanent secondary (emergency) access roadways will be provided where cul-de-sacs exceed 1,200 feet or when street stubs (such as the relocatable public roadway and access easement from the end of the cul-de-sac east to the property boundary as generally depicted on Figure 3 – Roadway Phasing Plan) to adjacent properties are not provided as required by the Reno Fire Department. Secondary (emergency) access roadways will include a 20 foot wide paved section within a 30 foot wide easement. These roads will be posted “No Parking” on both sides and will not be gated but will include signage indicating the use is for emergency access only. (Refer to Figure 9B - Secondary Access Street Sections).

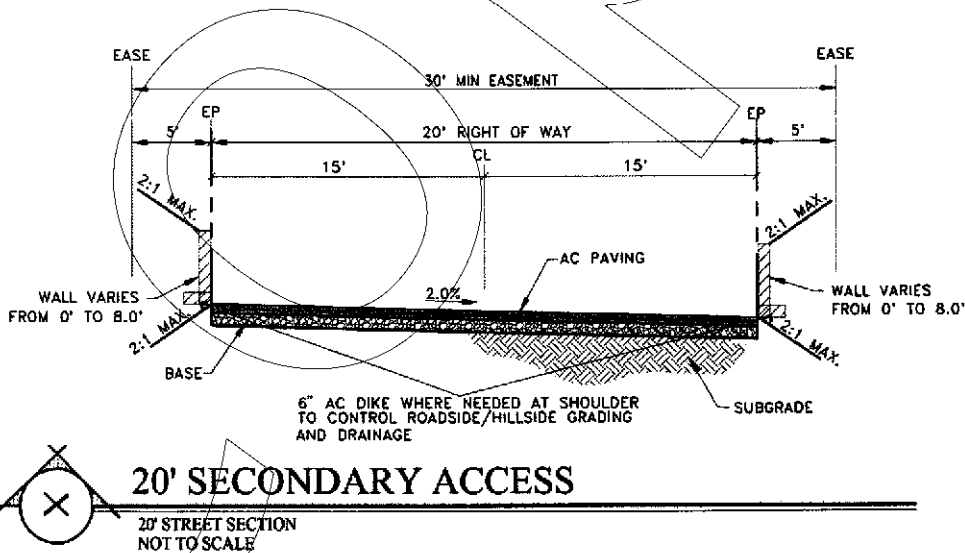


Figure 9B – Permanent Secondary (Emergency) Access Street Sections

Private Driveways and USFS Access

Private driveways will be provided to serve individual home sites in certain portions of the development and will be owned and maintained by the affected lot owners. In the southwesterly portion of the development four lots will be served by a private drive per the Reno Fire Department (lots 133 -136 as shown on the Tentative Map in the Appendix). Additionally, private driveways will serve three lots located in the southeast portion of the project. Private access roads providing access to tank sites and/or USFS land will be owned and maintained by the HOA. These driveways and access roads will include a 16 foot wide paved section and do not allow on-street parking (refer to Figure 10 - Private Drive and USFS Street Sections). The same 16 foot wide paved section will also be used to provide access points to the boundaries of lands located to the south and west of The Pines development and may also provide full or partial access to water tank sites as approved by the Community Development Staff. Where private driveways and/or USFS access routes are used for tank access, they shall include 16 feet of paved roadway with a 2 foot graded shoulder on each side for a total right of way width of 20 feet.

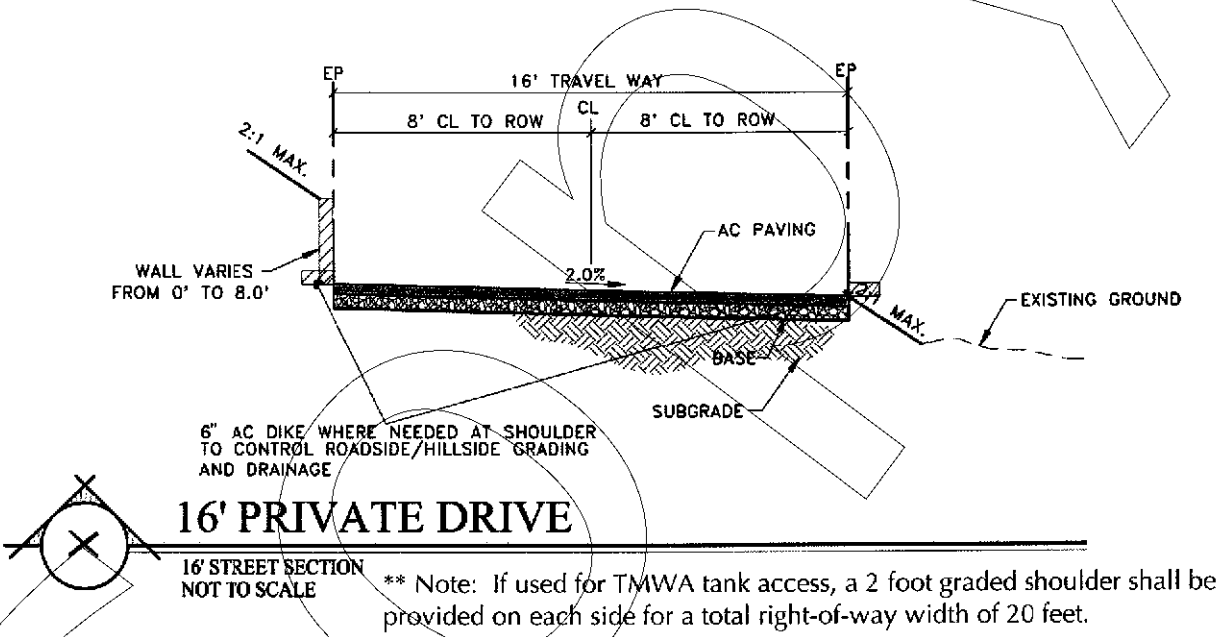


Figure 10 – Private Drive and USFS Street Sections

Fire Protection

Fire protection standards are outlined below:

- 1) The backbone loop road throughout the entire project shall meet City of Reno street grade standards.
- 2) Cul-de-sacs shall generally not exceed 1,200 feet in length, unless secondary emergency access (such as stubs to adjacent properties as shown for lots 237 – 248 on Exhibit A Tentative Map Grading Sheets in the Appendix) and its associated grading and access easements to adjacent properties are provided or other acceptable conditions are approved by the Reno Fire Department.
- 3) Fire access control gates restricting access to any of the public streets or any public access routes within The Pines project are not permitted. Gates will only be allowed to restrict access to TMWA water tanks or to individual lots or along private driveways where access to other lots will not be restricted.
- 4) Aside from bonding requirements, the City of Reno will not be associated with any activities, negotiations or regulation of how the owners/developers of The Pines or adjacent developers of the Quail Valley in The Pines project pay for the infrastructure improvements.
- 5) The developer will voluntarily contribute \$1,000 per lot at the time of approval of each final map to the Reno Fire Department for the purpose of improving or obtaining equipment for Fire Station 7.
- 6) Urban interface standards must be implemented in all phases of development. The standards will be required with each final map and must be approved by the Reno Fire Department and the Community Development Staff in accordance with the PSDS handbook.
- 7) Defensible space standards shall be required with each final map in accordance with the PSDS handbook. These will include such measures as fuel modification plans, fire resistant building materials, etc. Each final map must be reviewed and approved by the Reno Fire Department. Covenants, Codes and Restrictions (CC&R's) shall include the Defensible Space Wildlife Interface language found in the PSDS handbook.
- 8) The Pines will provide a 40 foot wide relocatable public roadway and access easement from the end of the northeasterly cul-de-sac in the project area to the property boundary to the east (owned as of the date of the PSDS by Evans Creek LLC) with the associated final map. It should clearly be understood that Evans Creek, LLC is not a participant in The Pines PUD and is not bound by The Pines PUD to provide or connect to any access in The Pines.
- 9) Paved private driveways, 16 feet in width, may be provided to serve a maximum of 4 lots on the southwesterly portion of the site (lots 133 – 136 as shown on the tentative map in the Appendix). In all other cases, paved, private driveways may only serve up to 3 lots.

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- 10) The developer will work with the USFS to accommodate access for fire fighting equipment staging and a parking area for up to 10 vehicles on the southeasterly portion of The Pines property boundary (Trailhead B) and parking for up to 7 vehicles on the southwesterly portion of the property (Trailhead A).
- 11) With each final map it shall be demonstrated that the project complies with all of the requirements in the Development Schedule Phasing and Fire Protection sections of the PSDS handbook.

Defensible Space/Wildland Interface

The Pines is located in a Sagebrush Steppe community, the driest and most widespread vegetative community in this region. Big sagebrush (*Artemisia tridentata*) is the most characteristic species. Mature pine trees and Mountain Mahogany shrubs are also located on the site. The risk of wildland fires in the Sagebrush Steppe community has increased since an exotic plant called cheat grass (*Bromus tectorum*) has become dominant. Cheat grass grows fast and then dries up creating perfect tinder for fires ignited by lightning.

With approximately 67% of the acreage in The Pines being kept as common open space, there is a potential threat of wildfires on the surrounding hillsides. As a defense against wildfires, final development plans for The Pines will conform to the requirements of the most recent edition of the Urban Wildland Interface Code as interpreted by the Reno Fire Department. Defensible space refers to a zone surrounding structures that is managed to reduce fire hazard. Within the defensible space, fuel modification measures, such as reducing fuel load and increasing the moisture content of the vegetation will be used to promote fire safe buffers. This zone serves as a physical barrier reducing the threat of wildfire and provides a visual transition between developed areas and open space.

Standards

- A landscape/open space maintenance and responsibility program for the common open space within The Pines and all individual lots will be established by the master developer, incorporated into the Covenants, Conditions and Restrictions (CC&R's), and implemented/enforced by the HOA as the properties are annexed into the CC&R's and become part of the HOA upon recordation of the final maps. The program shall be recorded with the first final map. The purpose of the program shall be to remove dried and dead fuel and any other combustible debris from the perimeter of any inhabited structure. The HOA shall be responsible for common open space areas while the individual lot owners shall be responsible for their lots (refer to Figure 11A - Defensible Space and 11B - Wildland Interface Concept).

- Paved parking areas can provide the required fuel buffer to homes, although in most cases the siting of parking areas should be such that they do not interfere with premium views from abutting homes.
- All residential structures shall use fire retardant roofing. In addition, non-combustible exterior building materials are encouraged.
- Persons owning, leasing, controlling, operating, or maintaining lots and/or homes or structures requiring defensible spaces are responsible for modifying or removing non fire-resistant vegetation on the property owned, leased or controlled by said person.
- Ornamental vegetative fuels or cultivated groundcover, such as green grass, ivy, succulents or similar plants used as groundcover, are allowed to be within the designated defensible space on each lot provided they do not form a means of readily transmittable fire from the native growth to any structure.
- An automatic irrigation system may be used to increase the moisture content of the living vegetation within the defensible space (fuel modification) area.
- Fuel modification shall be provided within a distance from homes or structures as specified in the table below and shall be the responsibility of each lot owner. Distances shall be measured along the grade from the perimeter or projection of the home or structure. The City of Reno Fire Marshall shall determine applicability and standards. In the event of a conflict between the ornamental landscaping requirement of the PSDS handbook and the requirements for fuel modifications, the fuel modification requirement shall prevail.

Table 2 - Required Defensible Space

Urban-Wildland Interface Area	Fuel Modification Distance (feet)
Moderate hazard	30
High hazard	50
Extreme hazard	100

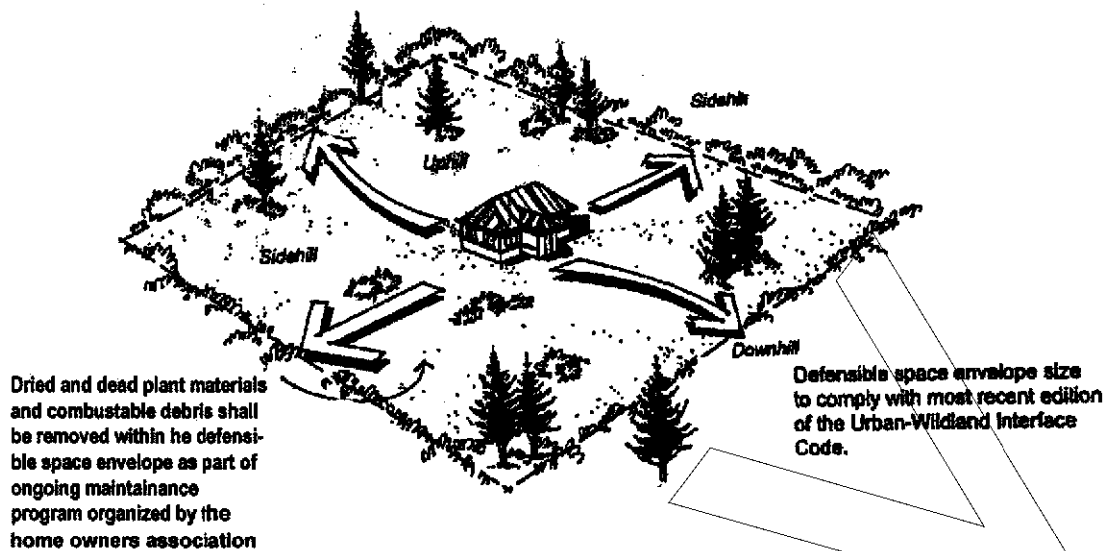
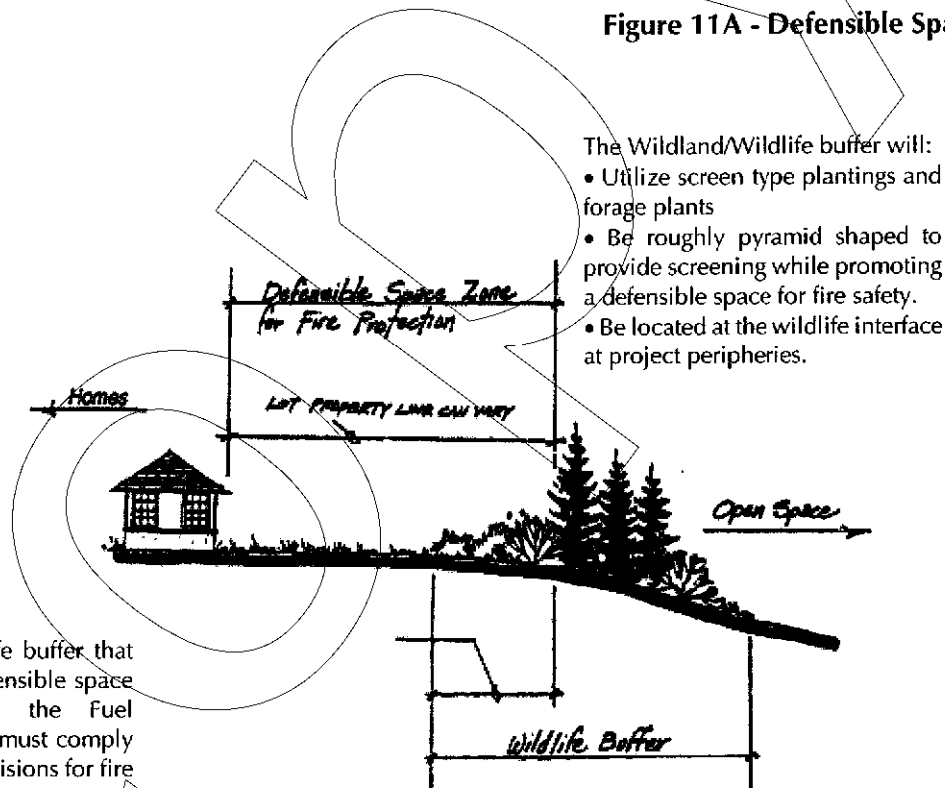


Figure 11A - Defensible Space



Portions of the wildlife buffer that encroach into the defensible space zone (described in the Fuel Modification Section) must comply with City of Reno provisions for fire safety.

Figure 11B- Wildland Interface Concept

Drainageways

Various drainageways are located within The Pines that convey runoff and will become part of this plan. However, the drainageways in general have been disturbed to some extent by roadway crossings and off road vehicle travel abuse that has occurred on the site for years. There are only two drainageways on the property that meet the criteria outlined in the City's Major Drainageway Plan to be classified as major drainageways. Roadway culvert crossings of the major drainageways will be limited to three locations (refer to Figure 12 – Major Drainageway Plan).

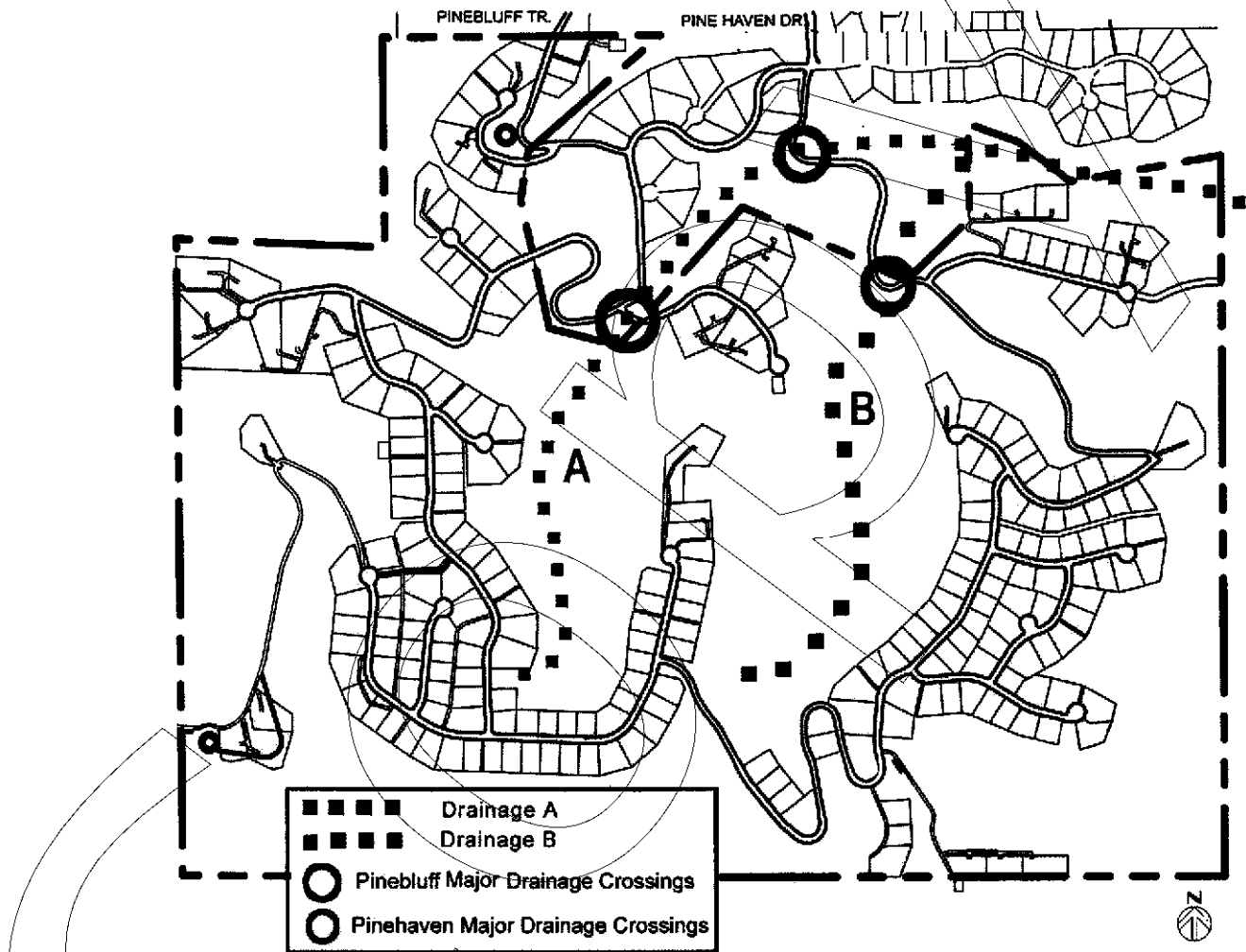


Figure 12 – Major Drainageway Plan

Minor drainageways may be modified, relocated, piped and crossed with roadways and trails in accordance with the PSDS handbook and as shown on the tentative map in the Appendix. Major drainageways however, may not be enclosed in pipes and may only be modified slightly to accommodate trails and roadway culvert crossings, if required and as approved through a special use permit in accordance with the City of Reno's "Major Drainageway Plan". A drainageway exhibit which identifies the extent of Drainageway Plan requirements and setbacks approved by Community Development Staff is included as an exhibit (refer to Exhibit C Drainageway Grade Break Setback map in Appendix). With each final map, it shall be demonstrated that residential building envelopes and all associated grading and construction disturbances for the lots shall not encroach into any designated major drainageways or their 15 foot setback as shown on Exhibit C Drainageway Grade Break Setback Map in the Appendix. It is important to note that two of the three crossings of the major drainageways will occur off-site on the Quail Valley in The Pines property. Where disturbance of any of the drainageways occurs, sufficient irrigation and landscaping to Community Development Staff's satisfaction will be provided in accordance with the PSDS handbook. This may include enhancements in the form of revegetation and/or ornamental landscaping as required in the PSDS handbook as applicable and as approved by the Community Development Staff. Each final map shall provide all necessary irrigation and landscaping which shall be reviewed and approved by the City's Landscape Architect.

When the Major Drainageway Plan was adopted by the City of Reno, it was recognized that from a recreational perspective, pedestrian trails would be functionally beneficial and add to the overall enhancement of the community. The current master development plan includes trails adjacent to and within the drainageway areas. These trails, as well as other equestrian and pedestrian trails will connect with the surrounding trails, USFS access points and the street sidewalk network at street grade locations. Where possible, existing jeep roads and other trails will be modified and incorporated into the overall trail system to limit disturbance to the site.

The major drainageways include the following minor modifications and enhancements:

Major Drainageway "A" (Western Natural Drainageway)

Major Drainageway "A" begins in the southwesterly portion of this project site. At-grade dirt roadway crossings of this drainageway currently exist downstream and off-site on the Quail Valley in The Pines property. The existing dirt crossings are located in close proximity where two roadway crossings (the Pinebluff Drainage Crossing and the Pinehaven Drainage Crossing) of Major Drainageway "A" are planned (refer to Figure 12 – Major Drainageway Plan). With the construction of the new roadway crossings, the existing dirt crossings will be scarified and revegetated and will result in an improvement to the site.

The Pinebluff Crossing of Major Drainageway "A" will be located beyond the main backbone road and is necessary to serve lots (lots 137 – 145) as shown on the tentative map in the Appendix) off of a cul-de-sac located at the north central portion of the site. Because this crossing is not along the backbone road (which requires additional "formal" landscape standards), this crossing will have enhanced landscaping. Special attention to this crossing will be provided as it is designated on Figure 21-Feature Areas as a feature area. All feature areas are required to include a minimum of three of the

following components: pine tree groupings, riparian vegetation groupings, rock (boulder) groupings, perennial flowers, entry signs, trail starting/ending points, public parking, USFS directional signage, split rail fencing, "bridge" treatments (railing and pilasters), benches, picnic tables, or trash receptacles. All pedestrian trails will cross the drainage at the street level. Revegetation and landscaping will be provided in accordance with the PSDS handbook. Roads to provide City access for maintenance will be combined with the existing trail system or designed in accordance with City of Reno requirements.

The Pinehaven Crossing of Major Drainageway "A" is located along the main backbone loop road. For this reason, the crossing will have a semi-formal appearance. Decorative railings with rock or rock face pilasters located at the "bridge" end points and every 25 feet will be provided along the crossing to produce a "bridge" look as approved by the Community Development Staff. Although this crossing is located on the Quail Valley in The Pines site, it may be appropriate to include project directional signage for The Pines at this location if permitted through easements by the owners of the Quail Valley in The Pines project. A minimum 4 foot wide by 6 foot high box culvert will be provided to allow ample space for the passage of runoff, reduce debris accumulation and allow for the passing of wildlife. The culvert headwalls will be enhanced with rock veneer or other similar treatment, as appropriate and as approved by the Community Development Staff. All pedestrian trails will cross the drainage at the street level. Revegetation and landscaping will be provided in accordance with the PSDS handbook (refer to Figure 13A – Pinehaven Major Drainageway Crossing-Typical Section and Figure 13B- Pinebluff Major Drainageway Crossing and other Minor Drainageway Crossings – Typical Section). Since this crossing is along the backbone road, "formal" landscaping standards (1 tree per 300 square feet with 6 shrubs per tree) will apply from the edge of the right-of-way out 20 feet with "informal" landscaping standards (1 tree per 600 square feet with 6 shrubs per tree) applying in disturbed areas beyond the 20 foot "formal" area. With the exception of the two culvert crossings, trails, landscaping and City of Reno required maintenance access roads Major Drainageway "A" will remain natural in its undisturbed state.

Major Drainageway "B" (Eastern Natural Drainageway):

Major Drainageway "B" also begins on the project site and is located to the east of Major Drainageway "A". An at-grade dirt roadway crossing also exists downstream near the boundary of The Pines and Quail Valley in The Pines properties. The existing dirt crossing is located in close proximity where the Pinehaven Drainage Crossing of Major Drainageway "B" will be provided (refer to Figure 12- Major Drainageway Plan). With the construction of the new roadway crossing, the existing dirt crossing will be scarified and revegetated and will, therefore, result in an improvement to the site.

The Major Drainageway "B" Pinehaven Crossing is also located along the main backbone road. This crossing will also have a semi-formal appearance. Decorative railings with rock or rock face pilasters located at the "bridge" end points and every 25 feet will be provided along the crossing to produce a "bridge" look as approved by Community Development staff. Since this crossing is located on The Pines site, it may be appropriate to provide project entry signage at this location. A minimum 4 foot wide by 6 foot high box culvert will be provided to allow ample space for the passage of runoff, reduce debris accumulation and allow for the passing of wildlife. The culvert headwalls will be enhanced with rock veneer or other similar treatment, as appropriate, and as approved by the Community

Development Staff. All pedestrian trails will cross both drainages at the street level (refer to Figure 13A – Pinehaven Major Drainageway Crossing-Typical Section). This roadway culvert crossing will be constructed in accordance with the PSDS handbook and may accommodate detention facilities if necessary (refer to Figure 13A – Pinehaven Major Drainage Crossing – Typical Section). Roads to provide City access for maintenance will be combined with the existing trail system or designed in accordance with City of Reno requirements. Revegetation and landscaping will be provided in accordance with the PSDS handbook. Since this crossing is along the backbone road, “formal” landscaping standards (1 tree per 300 square feet with 6 shrubs per tree) will apply from the edge of the right-of-way out 20 feet with “informal” landscaping standards (1 tree per 600 square feet with 6 shrubs per tree) applying in disturbed areas beyond the 20 foot “formal” area. With the exception of the two culvert crossings, trails, landscaping and City required maintenance access roads Major Drainageway “B” will remain natural in its undisturbed state.

Major Drainageway Crossings:

- Detention facilities shall be located in the minor drainageways on-site unless it can be determined that no other locations are suitable by the City Engineer. In such cases, detention areas may be approved in major drainageways by the Community Development Staff if deemed necessary on a selective basis and in compliance with the City's Major Drainageway Plan prior to implementation.
- Major Drainageways shall contain trails with at-grade street crossings as identified in the PSDS handbook (refer to Figure 16 - Trail Map).
- Roads to provide City access for maintenance will be combined with the existing trail system or designed in accordance with City of Reno requirements.
- All disturbances and/or improvements located/constructed within a major drainageway shall be in accordance with the City of Reno Major Drainageway standards and shall require approval of a subsequent special use permit by the City of Reno. No homes may encroach within setback areas outlined and approved by Community Development Staff in Exhibit C Drainageways with Grade Break Setback map in the Appendix unless a subsequent special use permit is approved. The map outlines the building envelope setback area including the 100 year floodplain and its 15 foot setback on each side of the drainageway or within any required setback area unless permitted through approval of a subsequent special use permit. No temporary or permanent disturbance of a major drainageway related to construction activities outside of the roadway culvert crossings, trails, and landscaping or City of Reno maintenance roads is permitted.
- With each final map, the master developer will obtain all required U.S. Army Corps of Engineers (USACOE) permits for drainageway activities and demonstrate compliance with the Wetlands and Stream Environments Ordinance. Any required permits from the USACOE shall be obtained prior to the issuance of any grading and building permits for each final map.

- Where a drainageway is disturbed, it shall be revegetated and landscaped in accordance with the PSDS handbook and as further required by the City's Landscape Architect.
- Rip-rap shall not be used except for storm drain armoring and bridge abutments and only when no alternative exists.
- The only channel modifications shall be for trails, detention areas, roadway culvert crossings, elimination of existing dirt crossings, City of Reno maintenance roads, revegetation or landscaping. Such modifications within a major drainageway (including the 15 foot setback) and its 100 year floodplain as identified and approved in Exhibit C Drainageways with Grade Break Setback found in the Appendix shall require approval of a subsequent special use permit by the City of Reno and shall occur only if absolutely necessary.
- All proposed landscaping and grading within the 100 year floodplain and 15 foot setback should be designed to be compatible with the existing condition, landscaped ornamentally for road crossings, or result in an improvement.
- Roadways crossing major drainageways (identified as the Pinehaven Drainage crossings on Figure 12 – Major Drainageways Plan) will include decorative rails to produce a “bridge” look as shown on Figure 13A – Pinehaven Major Drainageway Crossing – Typical Section. Roadways crossing major drainageways (identified as the Pinebluff Crossing on Figure 13B – Pinebluff Major Drainageways Crossing and other Minor Drainageway Crossings – Typical Section) will have an informal natural appearance. All three drainageway crossings are designated Feature Areas as shown on Figure 21- Feature Areas. Landscaping and grading will be in accordance with the PSDS handbook and designed to blend and transition to the surrounding channel and banks. Where disturbed, channel bottom landscaping shall be designed to equal or improve the existing drainage vegetation.
- Any project grading shall require a stormwater pollution prevention plan for any major drainageway disturbance or crossing. The plan shall include sediment control devices at the stormwater runoff points adjacent to construction areas and temporary silt fencing within 5 feet of all construction areas adjacent to the designated building envelope. Fencing and sediment control shall be removed after grading is completed and revegetation is applied.
- Common area, including major drainageways, trails, and associated culverts at street crossings on the site and possible detention facilities shall be maintained by the HOA. Off-site improvements shall be maintained by the HOA or in conjunction with the developer of the Quail Valley in The Pines project per a separate agreement between the property owners based upon “fair-share” development criteria. The City of Reno has no responsibility in negotiating separate agreements nor will the City be a party to any agreement.

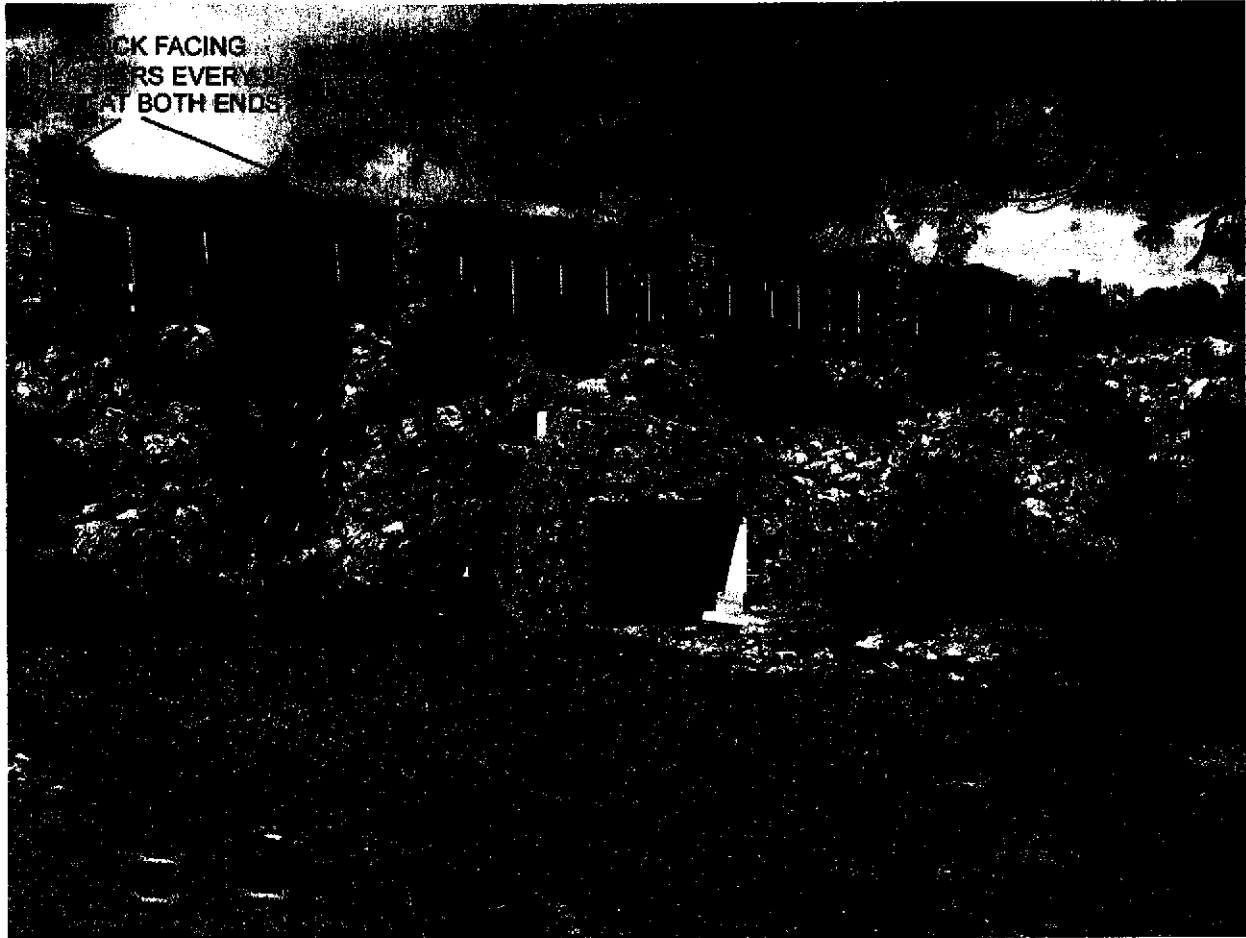


Figure 13A – Pinehaven Major Drainageway Crossing- Typical Section

Pinebluff Major Drainageway Crossings and Minor Drainageway Crossings:

- Minor drainageways and the Pinebluff Major Drainageway crossing of Drainage "A" as shown in Figure 12-Major Drainageway Plan may be modified, relocated, or piped where necessary and crossed with roadways and trails. Where the drainageways are not piped and roadway crossings or other modification requiring fill material are necessary, the design shall conform to the Public Works Design Manual and Figure 13B – Pinebluff Major Drainageway Crossing and other Minor Drainageway Crossings–Typical Section.
- The crossing design proposed in Figure 13B – Pinebluff Major Drainageway Crossing and other Minor Drainageway Crossings–Typical Section provides a method to limit the fill needed to place a roadway over a drainageway. With this approach to design, existing vegetation and slopes can be retained and preserved.

- Minor drainageways disturbed during construction of roads, trails and utility crossings will be revegetated and landscaped per the PSDS handbook. Undisturbed drainageways and common areas will be maintained in their natural state.
- Rip-rap shall not be used except for storm drain armoring and bridge abutments and only when no alternatives are available.
- Any project grading shall require a stormwater pollution prevention plan for any minor drainageway disturbance or crossing. The plan shall include sediment control devices at the stormwater runoff points adjacent to construction areas and temporary silt fencing within 5 feet of all construction areas adjacent to the designated building envelope. Fencing and sediment control shall be removed after grading is completed and revegetation is applied.
- Common areas including minor drainageways, trails, and associated culverts at street crossings on the site and possible detention facilities shall be maintained by the HOA. Off-site improvements shall be maintained by the HOA or in conjunction with the Quail Valley in The Pines project per a separate agreement between the property owners based upon "fair share" development criteria. The City of Reno has no responsibility in negotiating separate agreements nor will the City be a party to any agreement.



Figure 13B – Pinebluff Major Drainageway Crossing and Minor Drainageway Crossings – Typical Section

Other Stormwater Management Facilities:

- Prior to the approval of each final map or permit, the master developer shall have an approved Hydrology Report addressing on-site and off-site storm water flows and facility capacities for the pre-development and post development site conditions. Adequate easements and maintenance access shall be provided for all storm water management improvements per the Public Works Design Manual. All storm water management facilities shall be privately maintained.
- Storm water management facilities shall be installed with development in accordance with the PSDS Development Schedule and/or as needed for public infrastructure and site development improvements. Detention facilities shall be provided in accordance with City approved Hydrology Report submitted in support of each final map. Provisions shall be made on each final map allowing for private access and maintenance of private storm water management facilities located within public rights-of-way and easements. Adequate maintenance access shall be provided for all storm water management improvements in accordance with the Public Works Design Manual.

Site Planning Standards

A Special Use Permit shall be submitted and approved in accordance with the Reno Development code for the following:

- **Hillside Development**
- **Cuts greater than 20' and fills greater than 10'**
- **Disturbance of a Major Drainageway**

Allowed Uses:

- Single family dwellings of a permanent nature.
- Accessory buildings customarily incidental to the above uses, located on the same lot, including tool/storage sheds, garden houses, children's playhouses, private workshops and private greenhouses (aggregate square footage of accessory structures on each lot - 1,000 square feet maximum, 12 feet maximum overall height as defined in Reno Municipal Code (RMC). Location of accessory buildings shall not obstruct neighboring views as determined by the CRACC.
- Temporary sales office and model homes.
- In home child care for the number of children legally permitted with one caregiver.
- Structural additions to the original house and any other structure upon review and written approval by the CRACC, prior to submittal to City for building permits. The proposed structures must conform to setbacks and accessory structure size and height limitations established in this Handbook.
- Home occupations in accordance with RMC.

Prohibited Uses:

- No recreational vehicle, boat or trailer storage is allowed outside of garages. Pickup trucks with campers, vehicles or vans up to 20 feet in length that serve as primary transportation are exempt from this provision.
- Garages may not be converted into living space or used exclusively for storage and must maintain vehicle parking space(s), exclusive of a third garage space.

Minimum Yards/Building Setbacks:

Table 3 - Yard/Setback Standards

Minimum Lot Area:	.40 acre
Front Yard:	20'
Side Yard:	10' **
Rear Yard:	30' **

* Front yard setbacks shall vary for every other house where topography permits to reduce the monotony of repetitive setbacks. A minimum five (5') foot variation is required.

**20 feet if adjacent to common area unless property is restricted by the building envelope, by slope, or is deed restricted.

Building Siting/Envelopes:

It is anticipated that due to the special character of The Pines property, which provides outstanding views and a mountain "Tahoe" type environment on the site, that the majority of the homes will be architecturally unique and larger than average. Building siting is very important and will attempt to minimize disruption to the environment, protect important site features, and ensure that the use and enjoyment of the neighboring properties are not unduly impaired. Building envelope sizes and setbacks are required to be shown on all tentative maps and will be reviewed in accordance with hillside standards (refer to Figure 14-Hillside Building Siting Concept). Minor modifications to the building envelope sizes and configurations may be considered at the time of the final map to accommodate special site conditions or circumstances if approved by the CRACC and City of Reno Community Development Department.

Building Envelope - Evaluation Criteria

The master developer and City of Reno Community Development Staff will be responsible for determining final building envelope locations, setbacks and slope maps with each final map in accordance with the overall building envelope areas established with the tentative map in the Appendix and the PSDS. The lot owner's architect shall design each structure within the approved building envelopes. The owner of each lot shall obtain approval from the CRACC prior to submitting to the City of Reno Community Development Department for review and approval of individual building permits. The following building envelope criteria shall be considered.

- Fit of building plans to the terrain: Finished grades surrounding buildings should match the existing natural grades to minimize exposed cut and fill slopes.
- Degree of slope/topography in general: The more gentle a slope and/or overall topography is the more suitable an area is for building. Lots with gentler slopes will generally be able to have larger building envelopes which will be determined with the tentative map.
- Existing vegetation and natural features: The more vegetation and/or unique landforms or other natural features a lot has, the less suitable it is for building because retention of these features is integral to The Pines. Lots with these conditions will generally have more restrictive building envelopes which will be determined with the tentative map.
- Views from the proposed structure: Viewsheds from a building site should be retained to the degree possible.
- Effects on views from neighboring properties: When establishing building envelopes, the relationships between envelopes, where one building site can affect the views from other envelopes, will be considered. Separation from structures shall also be considered to ensure adequate spacing between homes. Each lot will contain a specific building envelope size equivalent to 80 feet by 80 feet to be placed within the overall building envelope area with the general goal of maintaining a 30 – 50 foot separation between adjoining structures. The envelopes shall meet all other provisions of the handbook such as restrictions on slopes, required setbacks, etc. Final building envelopes shall be recorded with the final subdivision maps.
- Retention of usable open space on each lot: Usable open space includes, but is not limited to

- minimum setback areas, areas that protect views, and areas that create space between homes, or between roadways and homes.
- Solar exposure and protection of solar access to adjoining properties: To the extent practical, the combined effect of envelope siting and height restrictions should be such that a building envelope will be in sunshine from 9 a.m. to 3 p.m. on December 21.
 - Vehicular access: Access to the building envelope should be such that the amount of land disturbance required for such access is minimal. Access shall meet City of Reno standards for driveway grades. Where appropriate, shared driveways are the preferred method for lots with slopes over 30% to minimize disturbance to hillsides. Driveways designed on slopes greater than 15% shall be shown on the tentative map.
 - Building setbacks: Building setbacks and envelope size and criteria shall be shown on the tentative map. A colored slope analysis map shall also be submitted with the tentative map and final maps indicating building envelope setbacks and non-disturbance areas. To the extent possible building envelopes shall be on slopes 20% or less. Lots containing slopes between 20%-25% shall be built at grade with disturbance limited to foundation and access construction only and shall utilize Hillside Adaptive Architecture exclusively. No structures will be allowed on slopes greater than 30%. Formal landscaping on lots with slopes 30 percent or greater shall be prohibited in order to preserve native topography and landscape except where areas abut street frontages where slope models and landscaping is required. The 30 percent areas shall be deed restricted to prohibit the landscaping as well as all out buildings, structures, fencing or grading disturbances.
 - Tree Removal: Pine trees over 12 inches in diameter shall be shown on each lot with each building permit. Pine trees over 12 inches in diameter proposed for removal for construction of an individual home, shall be reviewed by the CRACC. If required by CRACC, replacement shall be on a 1:1 basis with a 6 foot minimum evergreen tree provided by the individual lot owner either on the lot or within the common area feature areas. Trees 12 inches in diameter or greater that are retained due to creative and sensitive design will received a 2:1 credit to off-set tree replacement requirements outlined above.
 - Minor Deviations and Variances from the Approved Building Envelope after Recorded with the Final Map: The CRACC shall review variance requests from the approved building envelope on the final map and shall consider variance approvals in conformance with the criteria recorded in the Declaration of Protective Covenants for Caughlin Ranch. Any relocation of a building envelope must be submitted to Community Development Staff prior to issuance of any building permit to determine if it can be administratively approved (minor deviation) or if it requires review through the City of Reno variance procedures.

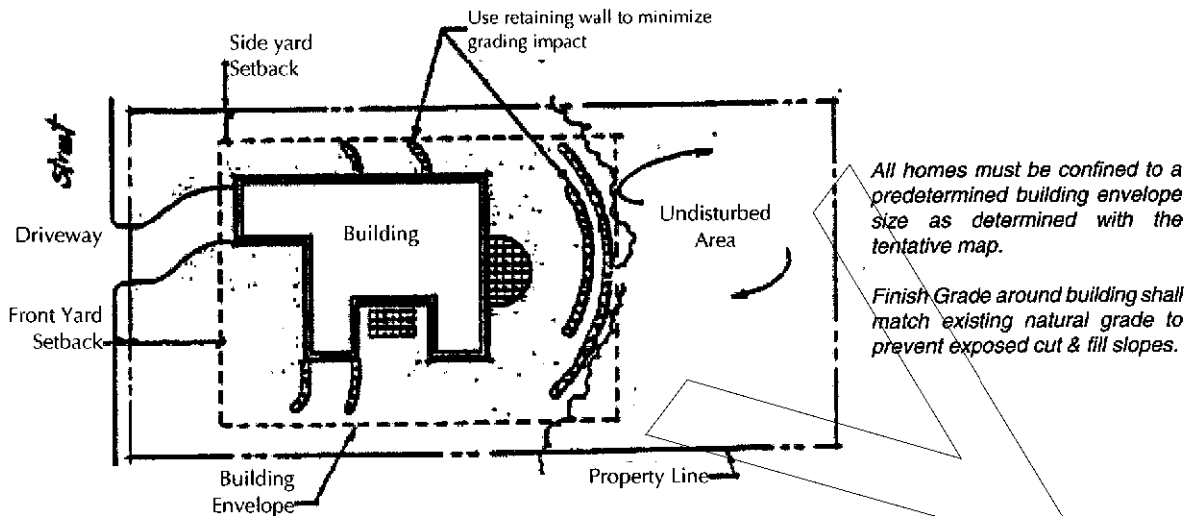


Figure 14 - Hillside Building Siting Concept

Projections into Required Setbacks:

- The main floor decks on all lots can project a maximum of less than 10% of the required rear setback as described above in Table 3 -Yard/Building Setback Section. Decks may be allowed on the second story.
- Second story decks/balconies may be permitted with the approval of the CRACC and the Community Development Staff provided privacy for neighboring residences is maintained.
- Cornices, canopies, eaves, chimneys, or similar architectural features may extend a maximum of two feet into a required front, rear, or side yard setback.

Accessory Buildings:

- Must be located within the building envelope and behind main structure, unless for a detached garage, which must be located 10 feet behind the front plane of the house.
- Rear and side setbacks shall be the same as the required side and rear setbacks of house.
- Maximum overall height, unless for a detached garage, shall be 12 feet measured from average grade around the building to the peak of the roof structure.
- Minimum distance from the main structure shall be 10 feet.
- Buildings shall be approved by CRACC prior to submittal for a building permit to the City of Reno. Architecture of accessory buildings must reasonably conform to the style of the primary residence.
- Maximum aggregate square footage shall be limited to 1,000 square feet of floor area.

Parking:

- Minimum two (2) car garage spaces and two (2) driveway parking spaces per house shall be required. In all cases, there shall be a garage space and/or on-site parking space for each bedroom in each house.

Height Limitation:

- The maximum ridge height for each house shall not exceed 36 feet from any point above natural grade. No portion of a home may exceed a true vertical height of the dimensions noted above for any location above the original (natural) grade. Building height is determined by measuring to the ridge of the roof (refer to Figure 15- Building Heights).

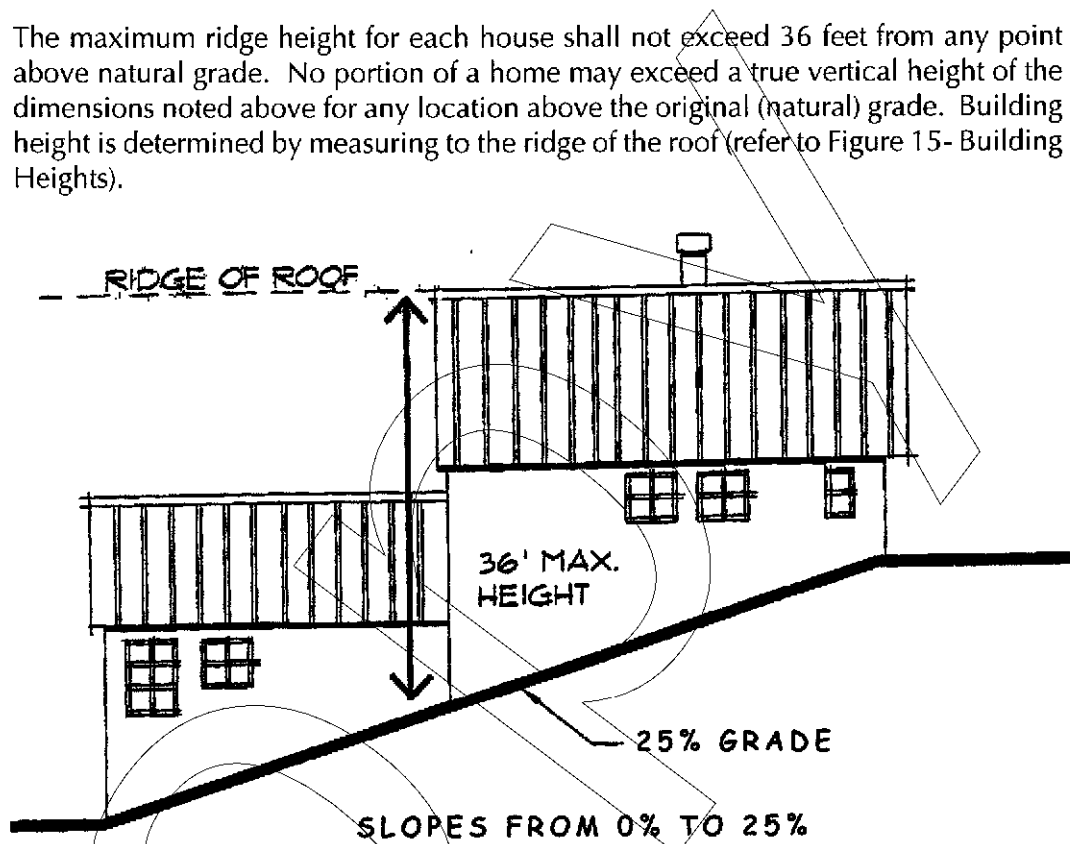


Figure 15 - Building Heights

Pedestrian and Equestrian Trails

Pedestrian trails and sidewalks will be provided throughout The Pines. Sidewalks will be provided on one side with development of all public streets and shall connect to trails throughout the development. An equestrian trail connecting to USFS property at the southerly property line and adjacent property to the east is also planned on the project site (refer to Figure 7A/7B – 30' Street Sections and 8A/8B – 40' Street Sections and Figure 16 – Trail Plan). Coordination regarding connections to USFS and trails that can be used by horses will be required and is subject to review and approval by the Community Development Staff and USFS.

The tentative map will show the pedestrian and equestrian trails in conformance with Figure 16 – Trail Plan. Final trail alignments are subject to approval by the Community Development Staff and Parks

Department, and where appropriate the USFS. Additional trails may be added by the master developer subject to approval by the Community Development Staff and the Parks Department. All existing jeep roads and trails not identified on Figure 16 – Trail Plan that are not specifically desired by the master developer will be scarified and revegetated so that a comprehensive, controlled trail system is developed on the site.

The following general guidelines will apply to all trails (refer to Figure 16 – Trail Plan):

- Trails will be shown on the tentative map per the PSDS handbook. At the tentative map approval, trails and phasing for construction shall be finalized to the approval of the Community Development Staff and the Parks Department.
- Trails will travel along the edges of open areas and in or above drainageways.
- Trail widths will vary from 2 to 4 feet based on topography in accordance with Figures 17A- Trail Type 1 and 17B- Trail Type 2.
- Trail types are identified as follows:
 - Trail Type 1: 4 feet in width with a compacted natural dirt surface (Refer to Figure 17A - Trail Type 1);
 - Trail Type 2: 2 feet in width with a compacted natural dirt surface; (Refer to Figure 17B –Trail Type 2);
 - Equestrian Trails: 4 feet in width with a compacted natural dirt surface. (Refer to Figure 17C – Equestrian Trail).
- Where feasible and practical, trails shall follow existing trails and jeep roads.
- Grades will follow existing topography where possible. Grades will normally be a maximum of 9% with the possibility of some segments having grades exceeding 20%, where topographic conditions dictate.
- Trails should avoid any wet meadow areas to minimize their impact and reduce the number of crossings that may be needed.
- Connection points to existing trails, USFS access roads identified on the tentative map in the Appendix and sidewalks will be made on The Pines property in accordance with the PSDS handbook.
- Trails will be kept separate from roadways where possible, with at grade street crossings of drainageways and other street crossings at controlled intersections, or at long tangent road sections. Signs and safety striping will be constructed where necessary and as determined by the Community Development Staff. At grade intersections between trails and streets will be clearly marked to advise both the motorist and pedestrian of approaching conflict. Signs alerting motorists will be placed in advance of intersections per the Manual of Uniform Traffic

Control Devices (MUTCD).

- On-site trails, trailheads and designated access points to USFS lands as shown on Figure 16 - Trail Plan shall be open to the public. On-site trails, trailheads and common areas will be owned and maintained by the HOA.
- Vehicle and pedestrian access points to USFS land shall be provided to the south and west boundaries of the property as shown on Figure 16 – Trail Plan. Access roads will be graded and paved in accordance with the development schedule outlined in the PSDS handbook.
- Upon submission of the tentative map, the applicant shall show all abutting pedestrian trails branching from the subject property. Prior to recordation of each final map, the trails shall be shown in detail, including their exact alignment, width, and materials. The Community Development Staff and Parks Department in consultation with USFS staff as appropriate shall approve the trail design in accordance with the PSDS handbook. Rock walls shall be used, as necessary, to ensure that grading disturbances do not extend more than 8 vertical feet above or below the pedestrian path/trail. Alignment, construction details and connections to roadways and other trails on site shall be provided to the satisfaction of the Community Development Staff and Parks Department. Off site improvements and connections on property not owned by The Pines shall not be the responsibility of The Pines.
- All trails and associated improvements shall be secured by a letter of credit or bond prior to the issuance of any permits for each final map that will trigger the trail improvements.

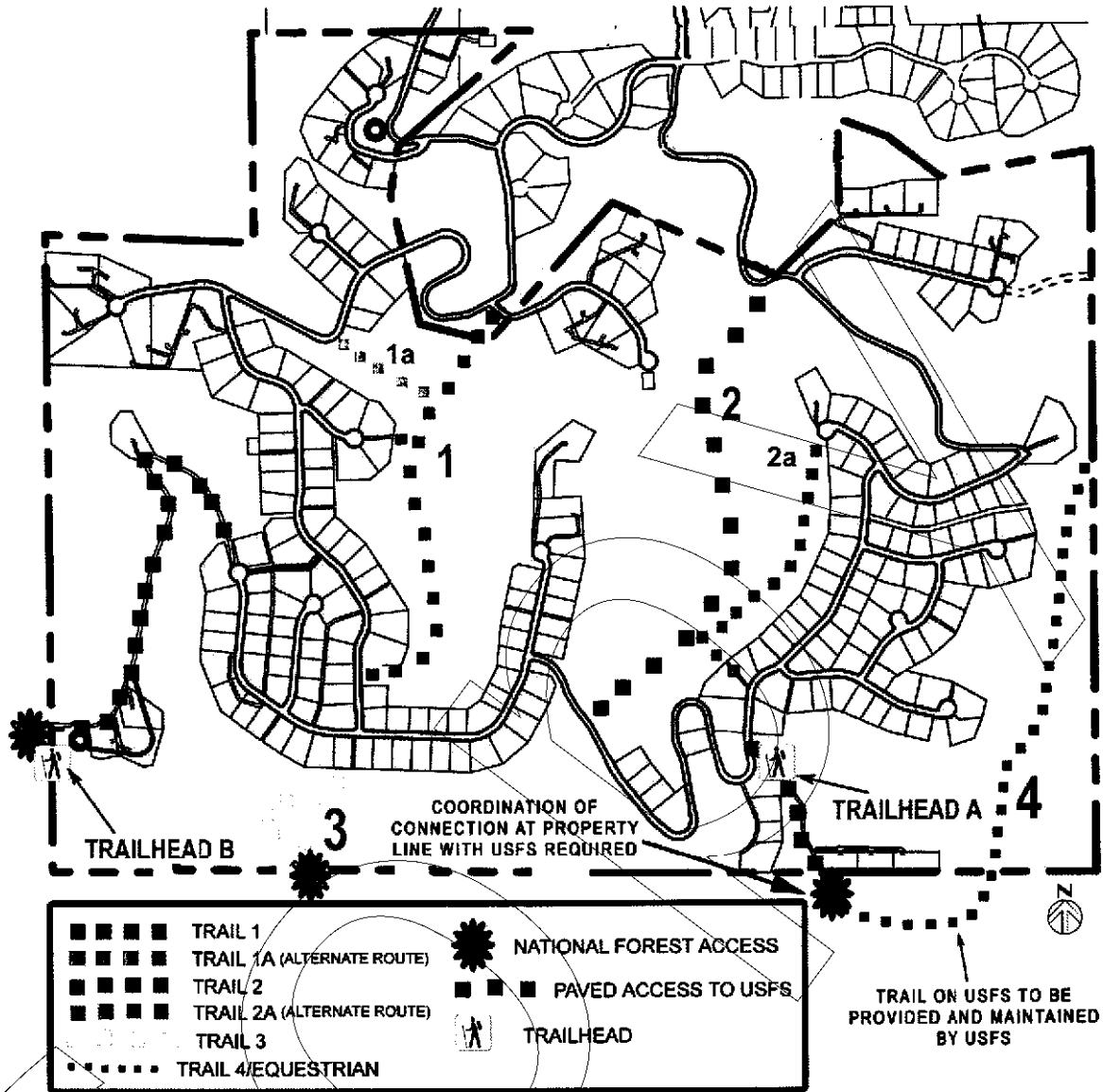


Figure 16 - Trail Plan

Trail Type 1

- Trail type is intended for flatter portions of the site.
- To be constructed within or adjacent to major drainageways.
- Trail widths will be 4 feet wide.
- Trail is intended to be built into native conditions to the extent possible.
- Trail will avoid excessive grading and disturbances as appropriate.
- Trail to be constructed with compacted natural dirt.
- On-site trails to be constructed by the master developer and maintained by HOA (off-site trails are the responsibility of others).

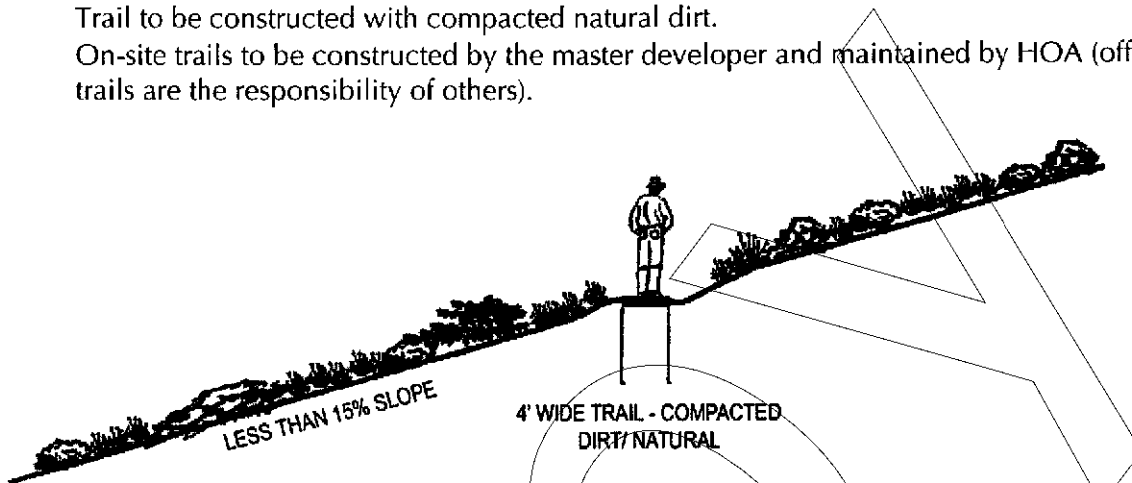


Figure 17A – Trail Type 1

Trail Type 2

- Trail type is intended for accessing steeper sections of site (15% or greater).
- Trail widths will be a minimum of 2 feet in width.
- To be constructed within or adjacent to major drainageways.
- Trail to be constructed with compacted natural dirt.
- Intended to be built into native conditions to the extent possible.
- Trail will avoid excessive grading and disturbances as appropriate.
- On-site trails to be constructed by the master developer and maintained by HOA (off-site trails are the responsibility of others).

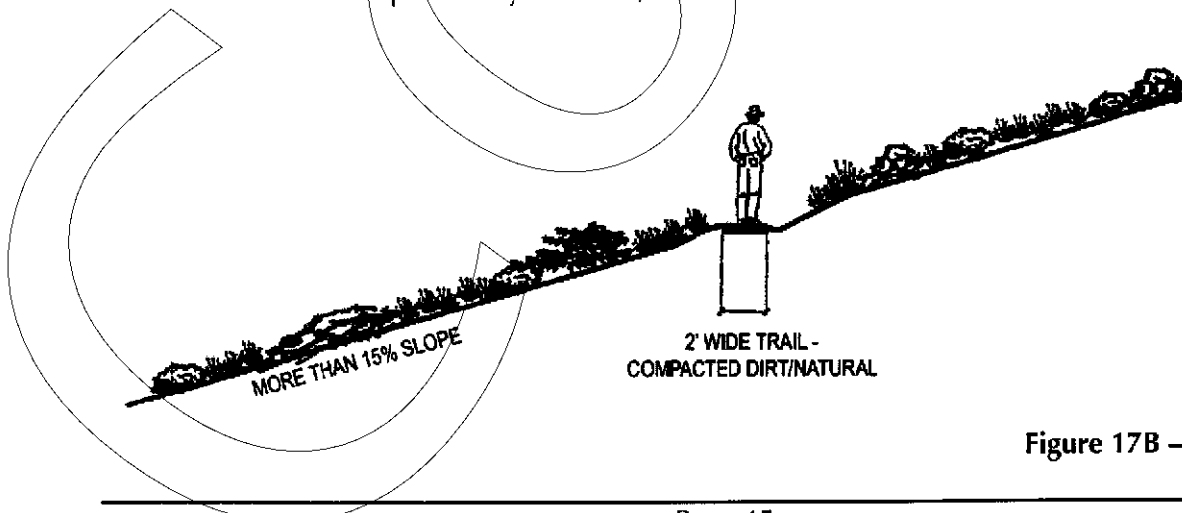


Figure 17B – Trail Type 2

Equestrian Trails

- Trail widths will be a minimum of 4 feet wide.
- Trail is intended to be built into native conditions to the extent possible
- Trail will avoid excessive grading and disturbances as appropriate
- Trail to be constructed with compacted natural dirt.
- On-site trails to be constructed by the master developer and maintained by HOA (off-site trails are the responsibility of others).

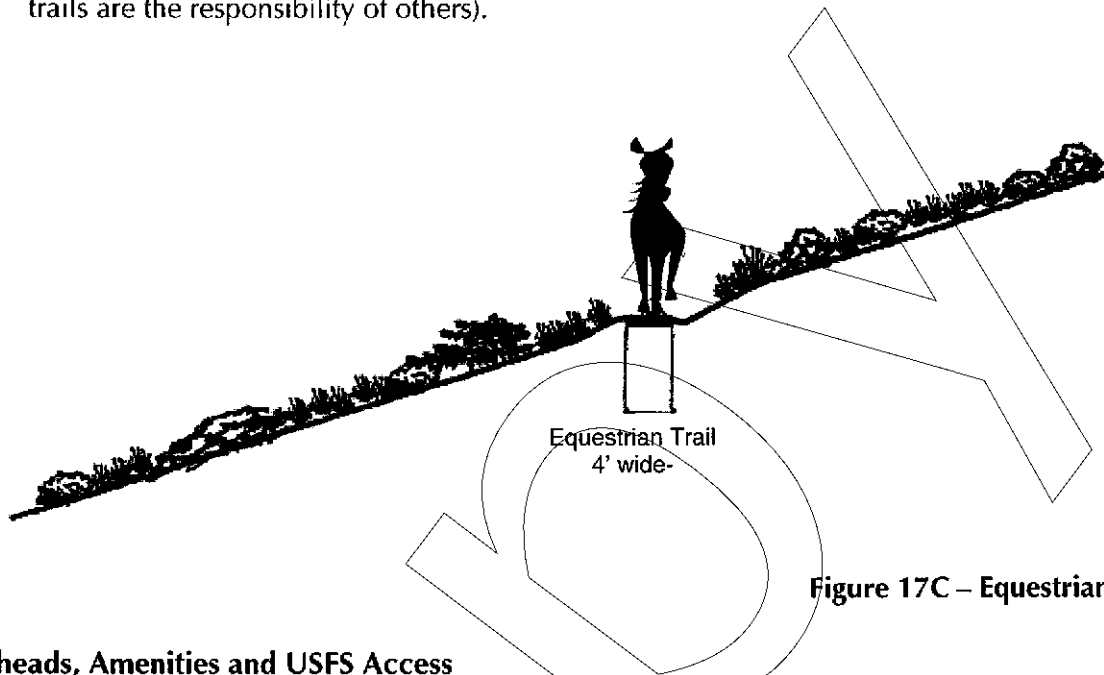


Figure 17C – Equestrian Trail

Trailheads, Amenities and USFS Access

Public access to USFS lands to the south and west beyond the boundaries of The Pines is important. Two trailheads shall be installed on The Pines site to facilitate parking, recreational access and fire fighting equipment and access to USFS property. The location of the trailheads and the amenities provided shall be approved by the USFS and the Community Development Staff with the tentative map. In addition, two historic vehicular access points at the southwesterly boundary of the site (to Hunter Lake) and at the southeasterly portion of the site (to USFS points south) will be retained and improved with a 16 foot wide paved roadway in accordance with the street sections and development schedule in the PSDS handbook (refer to Figure 16 - Trail Plan). In order to direct and control public access on the property, all existing dirt jeep roads, miscellaneous trails and access points not identified in Figure 16 - Trail Plan will be scarified and revegetated.

A minimum of four small directional signs indicating access to the two USFS trailheads will be provided along the roads in The Pines in accordance with Figure 25 - Sign Plan. Prior to the approval of the first final map, the master developer will be required to submit a disclosure statement to the Community Development Staff that will be signed by every lot purchaser, which notifies the lot purchaser that all roads, paths and trails within The Pines and the historic access points at the southwesterly and southeasterly portions of the site as shown in Figure 16 – Trail Plan will remain open and public, and that vehicular access and trailhead parking for fire suppression and public use and

THE PINES

enjoyment will be improved/installed and be perpetually open and maintained without the installation of gates.

Trailhead A will be designed as the major trailhead and is located at the southeasterly portion of the site (refer to Figure 18 – Trailhead A). This trailhead will also serve as a staging area for the USFS in the event of a wildland fire and will accommodate turning criteria for fire fighting equipment. The trailhead will include ten parking spaces and will be paved. This area has been designated as a Feature Area on Figure 21 – Feature areas. Amenities at this location will include three picnic tables, three trash receptacles, and landscaping in accordance with the PSDS Common Area Informal Landscaping Standards.

This trailhead will also include trailhead signage (bulletin board format) as approved by the USFS, the Parks Department and the Community Development Staff. Trail maps and fire/safety information/restrictions for hikers and off-road vehicle users will be obtained from the USFS and posted at the trailhead.

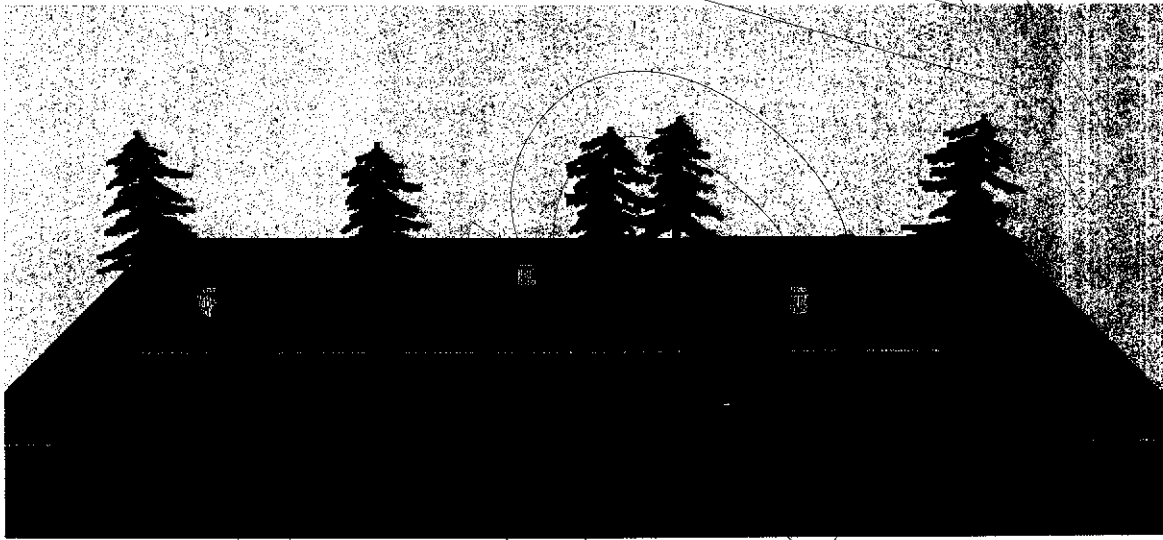


Figure 18 – Trailhead A

Trailhead B is located at the southwesterly boundary of the site. This trailhead will include a minimum of seven parking spaces, two picnic tables and two trash receptacles. The parking area will be paved. In addition, trailhead signage (bulletin board format) as approved by the USFS, the Parks Department and the Community Development Staff will be provided. The type of landscaping and revegetative seed mix used for the tanks and booster stations shall be determined based on site conditions at the final locations for the structures as determined by TMWA. Consideration shall be given to existing trees, how close the utility is to proposed homes, and whether construction of the utility will require the elimination of existing mature trees on site. Temporary irrigation shall be provided as necessary. The CRACC and the City's Landscape Architect shall review and approve the final plans for these utilities with each associated final map. All on-site trails, trailheads and amenities will be owned and maintained by the HOA.

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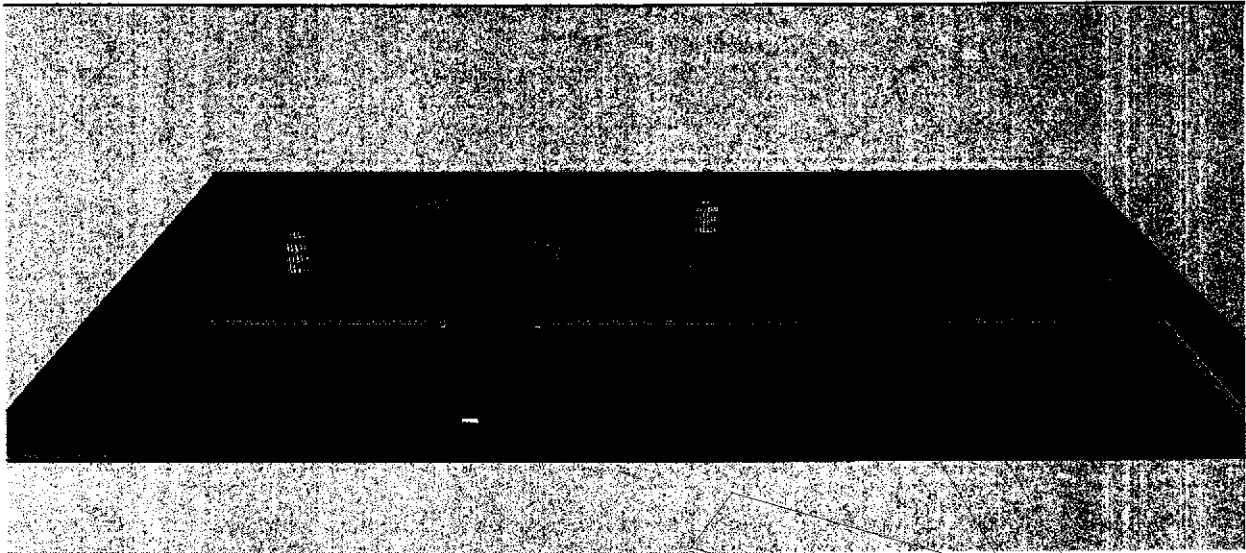


Figure 19 – Trailhead B

Trail and amenity phasing will be provided as follows:

- Prior to the issuance of a certificate of occupancy for the 25th lot, Trailhead A as shown on Figure 16, Trail Map and the associated improvements outlined in the PSDS will be constructed. Paving of the trailhead parking area, however, will occur when the road providing access to the trailhead is paved as outlined in the development schedule in the PSDS handbook.
- Prior to the issuance of a certificate of occupancy for the 50th lot, the two primary trails adjacent to the major drainageways identified as Trails 1 and 2 and the equestrian trail identified as Trail 4 on Figure 16, Trail Map will be constructed.
- Trailhead B and the associated improvements outlined in the PSDS and other trails identified as Trails 1a, 2a and 3 as identified on Figure 16 - Trail Map will be constructed prior to issuance of a certificate of occupancy for the 75th lot. Paving of the trailhead parking area, however, will occur when the road providing access to the water tank is paved as outlined in the development schedule in the PSDS handbook.

Architecture

Architectural standards will refer to Caughlin Ranch Development Standards handbook for direction to ensure the highest quality architecture in The Pines. Acceptable building materials and colors will be consistent with those in the approved Caughlin Ranch Development Standards handbook as well.

Architectural Policy & Control

All buildings must incorporate the "Caughlin Ranch" theme or flavor architecturally, in a manner that is complementary and compatible with the site and its surroundings. All building proposals must be prepared by a licensed architect and will be reviewed by the CRACC to ensure this policy is enforced in a way that encourages creative design and considers the specific site conditions involved. Before final CRACC approval of an individual residence is granted, the property owner must execute an improvement agreement consistent with CRACC policy to insure that each building is completed as approved by the CRACC.

Architectural Standards

- All houses shall be finished on all four sides with regard to rooflines, windows, and architectural features.
- Houses shall be designed as custom and semi custom homes. In general, semi custom homes may only be located outright on the two plateaus on the site as shown on Figure 20, Permitted Locations of Semi-Custom Homes. Semi custom homes, however, may be approved off the plateaus on an individual basis for home sites that have slopes of 20% or less within the building envelope for the lot with approval of CRACC and the City of Reno Community Development staff. No repetitive semi custom homes will be allowed on the site. Floor plans, elevations, finishes, roof lines, etc shall be demonstrated to be substantially different from one another. No production/tract homes shall be permitted in The Pines.
- With application for each building permit, the CRACC must approve the plans and provide a letter of approval to the City of Reno.
- Minor adjustments to the architecture which are consistent with the project standards may be allowed with approval by the CRACC and the Community Development Staff in accordance with the PSDS handbook.

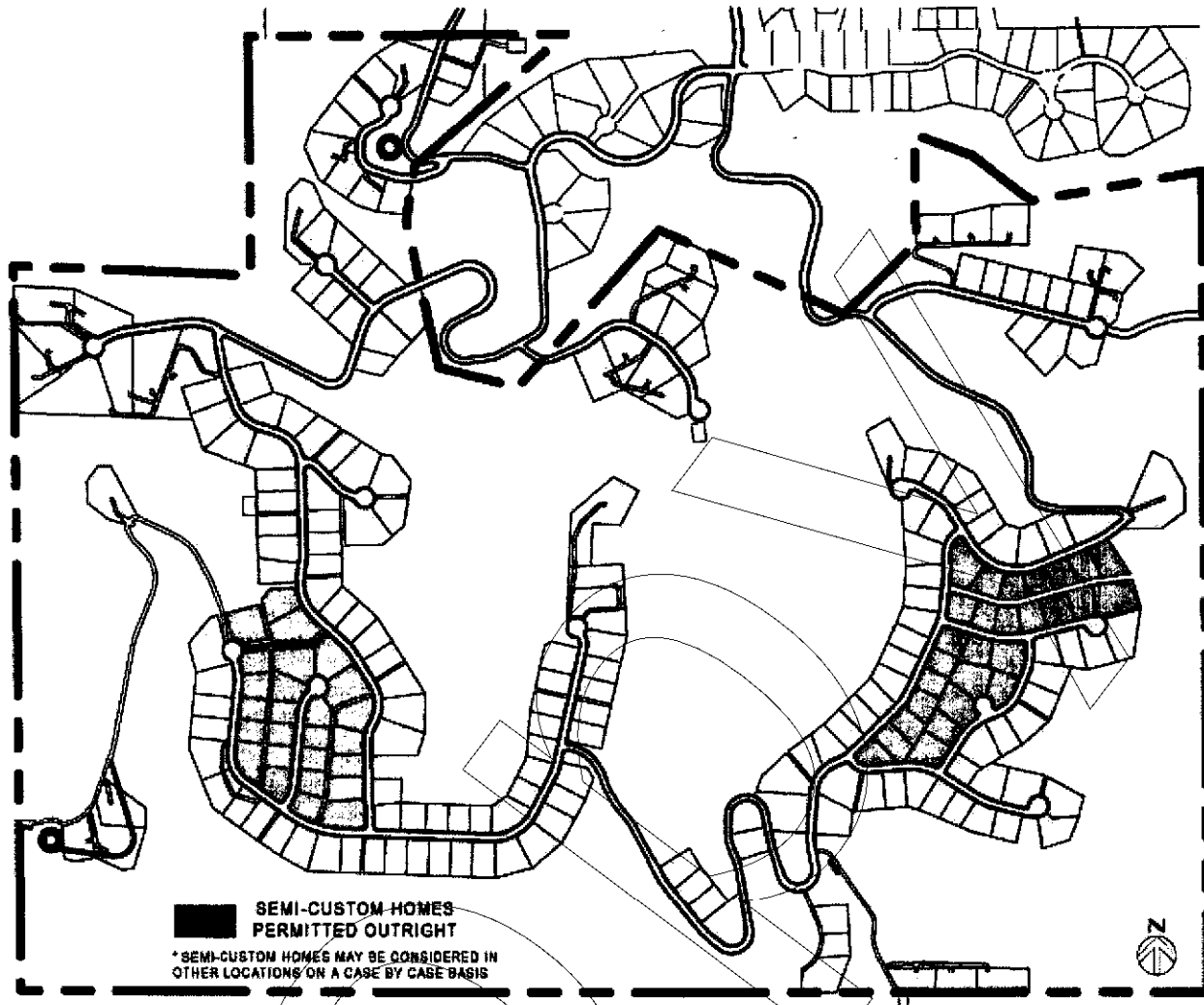


Figure 20 Permitted Locations of Semi Custom Homes

Hillside Adaptive Architecture

Hillside grading techniques to minimize site disturbance will be implemented. Also, hillside adaptive architecture will be used to provide architecture that integrates with the terrain. Specific hillside architecture will be required for all lots with average slope of 20% or greater in the building envelope. The intent is to fit house architecture to the natural topography to minimize grading disturbances to the natural terrain. The grading concept for The Pines lots is to custom grade the majority of the lots, however, some lots will be "pad" graded as determined through the tentative map process (refer to Figure 21A – Hillside Adaptive Architecture/Uphill Concept; Figure 21B – Hillside Adaptive Architecture/Downhill Concept; and Figure 21C – Hillside Adaptive Architecture/Sidehill Concept).

All lots with disturbed slopes greater than 3:1 shall be treated in accordance with the methods described in Table 4 – Slope Treatment Models. Exposed cut slopes, such as those along streets, driveway accesses, or yard areas greater than eight feet in height shall be terraced and retained with rockery walls no higher than eight (8') feet in the front yard setback. Walls, however, may be required to be lower depending on line of site distances. Terraced widths shall be a minimum of eight (8') feet to allow for the introduction of vegetation for erosion control, and to accommodate trees. Smaller widths will be allowed on a case by case basis, and only where absolutely necessary, and shall be approved by the Reno Community Development Staff. Vegetation shall be installed in such a manner as to be substantially established within one year of installation.

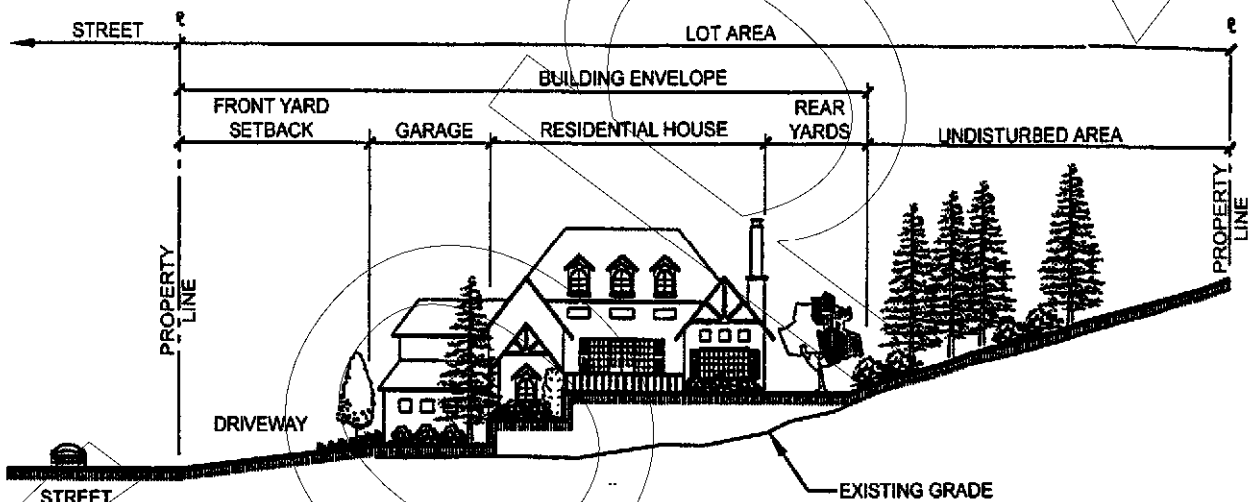


Figure 21A - Hillside Adaptive Architecture /Uphill Concept

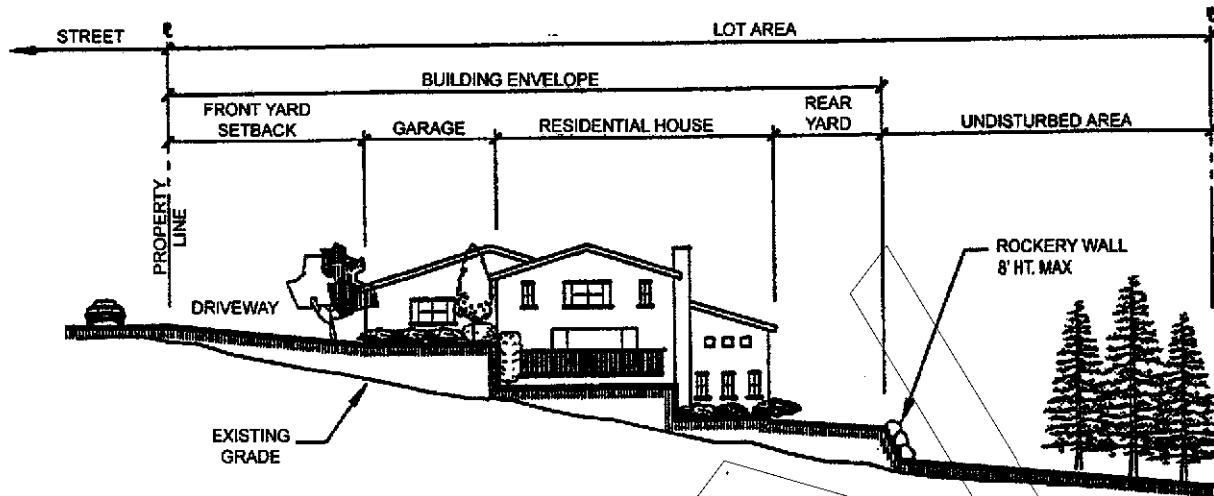


Figure 21B - Hillside Adaptive Architecture /Downhill Concept

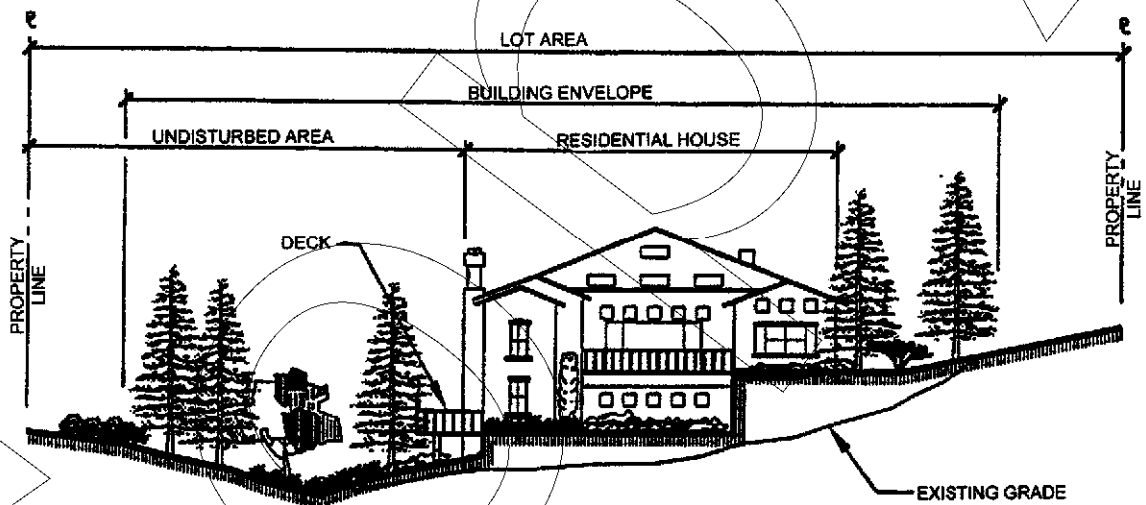


Figure 21C - Hillside Adaptive Architecture /Sidehill Concept

Grading and Slope Treatments

Landscape Slope Treatments (4 Models)

The Pines is situated on slopes that provide an opportunity for a variety of graded slope treatments. Existing vegetation on the site varies depending on location and includes mature pine trees in moderately dense groupings, Mountain Mahogany brush, sagebrush, as well as exposed areas with limited vegetation. Depending on the gradient, disturbed slopes will be planted with one of the proposed planting models as illustrated below. Four slope treatments or models will be used depending on the existing slope condition and shall be shown on the tentative map. These methods are used throughout the site and used in combination to provide visual interest, variety of planting concepts, and to allow the developer flexibility in providing slope stabilization. All four of the slope treatment models that follow will be used in accordance with the standards shown in Table 4, Slope Treatment Models. Use of a particular standard, however, should also be based on whether or not the treatment will be permanent or temporary. Initially temporary treatments such as revegetation and erosion control in accordance with City requirements will occur along the backbone loop road where there are no lots. Permanent treatment of the formal and informal landscaping in accordance with the PSDS handbook will be provided along the backbone roadway as it is paved in accordance with the development scheduled in the PSDS. Temporary treatments may also be used during development of the backbone loop road where there are adjacent lots that will be custom graded at a later date when individual homes are constructed. Where these conditions exist, it will be important not to use a landscape slope treatment that will restrict lot development, create difficult building challenges, impair architectural creativity or require removal of costly landscape improvements.

Application of each slope treatment model depends on the severity of the natural grade. The intent is to require a more stringent grading standard as the natural grade increases to reduce the area of disturbance and to have continuity along roadways where the model chosen is a "best fit" to the dominant slope condition in a given development area. Further, it is intended to avoid staggered, short segments, or isolated changes in the treatment types that would not blend with the natural environment. The Community Development Staff has the discretion to determine along with the master developer which slope standard will best meet the intent based on unique or extraordinary circumstances at a location. These standards will be determined with the tentative map (refer to Figures 23A – 40% Natural Slope through Figure 26B – Hydroseed Slope Treatment).

Table 4 - Slope Treatment Models

Average Natural Slope	Slope Treatment Type			
	Model 1 Rockery Wall	Model 2 Planted Top Rock	Model 3 Planted Talus Rock	Model 4 Hydroseed Application
0 to 20%	X	X	X	X
>20% to 30%	X	X	X	X ⁽¹⁾
>30% to 40%	X	X	X	
>40%	X			

1) Requires erosion control matting as directed in the geotechnical report

- Notes: A) Each of the boxes marked with an X is a permitted option for a slope range
 B) Slope model will be selected with the tentative map in accordance with a slope analysis map for a respective development area. Based on a more detailed analysis at the final map stage, Community Development Staff may require that a different slope model be used other than that shown on the tentative map. Changes will be based on road re-alignment, existing vegetation or other design considerations.

Common Area Tree and Shrub Planting

Application

These standards apply to the common areas along the backbone loop road where there are no lots fronting the roadway and along other roadways where disturbance occurs where there are no lots fronting the roadways. Where lots are adjacent to the disturbed areas, temporary slope treatment will be provided by the master developer or successor declarant in accordance with the PSDS handbook. Final landscaping and final slope treatment of each individual lot shall also be in accordance with the slope treatment and individual lot landscaping standards in the PSDS handbook and shall be approved by the CRACC and installed by the individual homeowners and/or builders as each lot is developed.

Intent

Due to the existing natural vegetation and character of The Pines property, it is not the intent to provide manicured landscaping or tree lined streets in the development. Instead, the intent is to enhance natural vegetation and site features when restoring areas graded for roads and slopes. Clustering of trees and shrubs shall be required to accentuate site features such as entrance areas, key intersections, the primary trailhead and decorative drainageway crossings (refer to Figure 22 - Feature Areas). Feature areas are intended to be small areas (generally 100 square feet) of distinguished visual interest. As the environment associated with each feature area varies due to its location and function, where appropriate Feature Areas will include a minimum of three of the following components: pine tree groupings, riparian vegetation groupings, rock (boulder) groupings, perennial flowers, entry signs, trail starting/ending points, public parking, USFS directional signage, split rail fencing, "bridge" treatments (railing and pilasters), benches, picnic tables, or trash receptacles at the discretion of the master developer and approved by Community Development Staff. The City's Landscape Architect shall have the discretion to reduce the number of trees and shrubs, and adjust their locations and sizes, should the site conditions and topography warrant. For example, rock outcroppings, existing tree

clusters, shrubs, drainageway features, plateaus, hillsides, and other natural site characteristics shall be respected. The landscaping standards are designed to work with all four slope treatment models as outlined in Table 4 – Slope Treatment Models.

Standards

- Two types of permanent landscape treatments will be required and are described as “formal” and “informal”.

1. “Formal” Landscaping Standards:

These standards shall apply and extend from the edge of the right-of-way out 20 feet along the common areas of the backbone loop road where there are no lots fronting the road and shall be installed in accordance with the development scheduled outlined the PSDS handbook. When rockery walls are placed within the 20 foot formal landscape area, the 20 foot measurement will include the planting areas created within the rockery wall terraces. Any area of disturbance beyond 20 feet may utilize the “informal” landscaping standards.

The 20 foot wide common area of the backbone loop road shall be landscaped with trees planted at a density of 1 tree per 300 square feet with six native or ornamental shrubs per tree.

Tree and shrub sizes shall be as follows:

Tree mix: 80% evergreen, 20% deciduous

Trees: Evergreens 20% at 3 feet; 60% at 6 feet, 20% at 8 feet

Deciduous – Per Reno Municipal Code (RMC)

Shrubs: 75% 1 gallon, 25% 5 gallon

2. “Informal” Landscaping Standards

These standards shall apply only to disturbed common areas beyond the 20 foot “formal” landscape area along the backbone loop road and disturbed common areas along all other roadways.

Planting in these disturbed areas shall include 1 tree per 600 square feet with slopes revegetated with native seed mix with six native shrubs per tree at 1 gallon sizes. Revegetation shall be in conformance with Chapter 3 of the Public Works Design Manual including temporary irrigation. All revegetation areas shall include a seed mix that includes wildlife forage seeds. All trees in the informal landscaped areas shall be evergreen with 20% at 3 feet and 60% at 6 feet, and 20% at 8 feet.

Pine trees over 12 inches in diameter shall be shown on each final map where roadway and infrastructure improvements are planned. Trees within rights-of-way and proposed disturbed areas removed during construction of the backbone loop road shall be replaced with 6 foot high trees at a ratio of 1:1 and may be placed throughout the common areas of the site to enhance entrance areas, key intersections, the primary trailhead and decorative drainageway crossings, where irrigation can be provided and

THE PINES

as appropriate. Where trees 12 inches in diameter are saved in disturbed areas due to grading adjustments and creativity, along the backbone loop road, a 2:1 tree credit to off-set tree replacement requirements above will be applied.

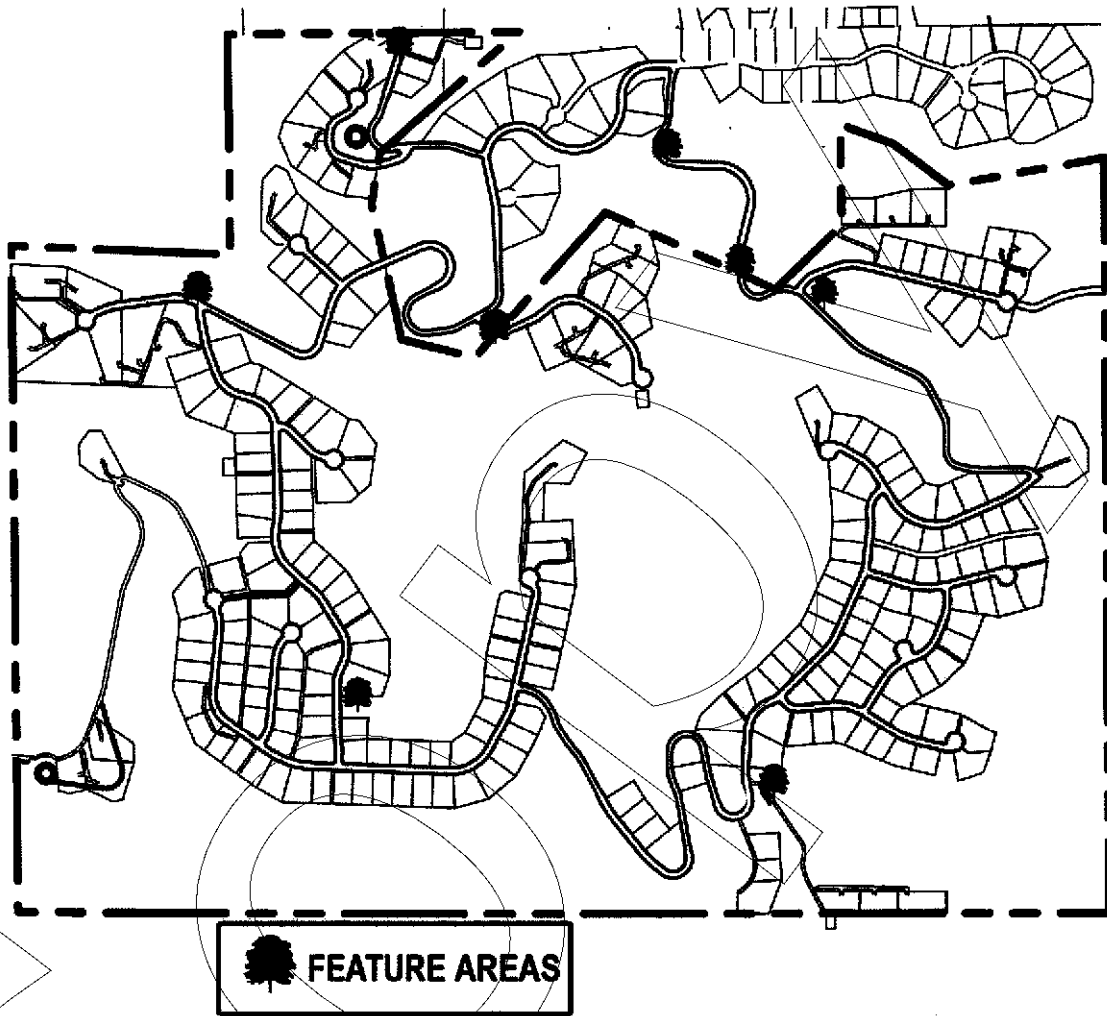


Figure 22 Feature Areas

Terraced Rockery Wall (Model 1)

The rockery wall model is intended to be used to limit the removal of mature trees to reduce excessive slopes and to limit the disturbance area for the installation of trails in and along the major drainageways.

This wall concept uses walls with benched areas of between 8 and 10 feet wide including drain rock. If approved by the Community Development Staff, benches may be less than 8 feet wide to accommodate topographic constraints where it results in less disturbance. Benches less than 8 feet may only be used where rockery walls taper and where they can be maintained safely. Landscaping will consist of a mix of deciduous and evergreen trees and shrubs that soften the wall with both upright growth and narrow cascading branches that "drape" down the front of the walls. In sections where the benched area is less than 8 feet in width, shrubs or columnar trees such as Pines, Junipers, and Arborvitaes species will be planted as approved by Community Development Staff. In sections where the benched area is greater than 8 feet in width, the full plant palette included in the Appendix should be utilized. Typically the plants will be grouped and adequately spaced for a natural appearance. This type of slope treatment will be generally located along the most severely graded slopes, but is allowed for all slope mitigation (refer to Figure 23B - Terraced Rockery Wall). Permanent bare rock cut slopes steeper than 2:1 may be used if approved by a geotechnical engineer and the Community Development Staff.

Application

This slope treatment will be required when the average natural grade for the cross slope being graded is over 40% and the proposed grade on the cross slope (cut or fill) ranges from 2:1 to 1:1 (refer to Table 4 – Slope Treatment Models and Figures 23A – 40% Natural Slope and 23B – Terraced Rockery Wall).

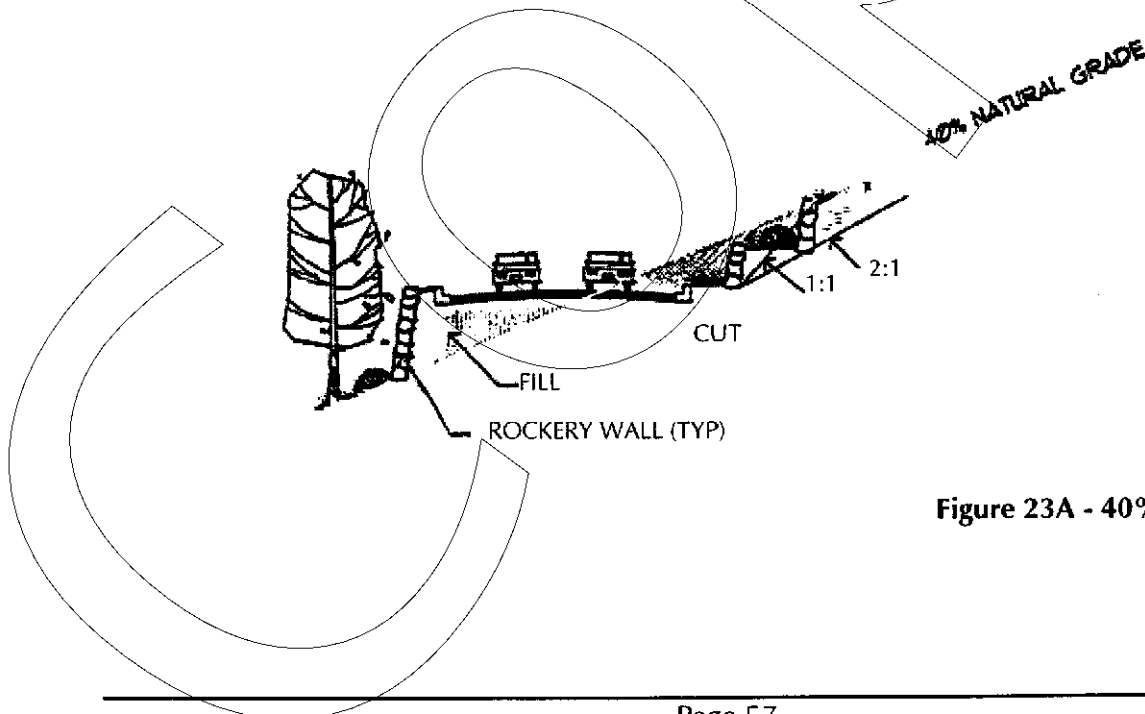


Figure 23A - 40% Natural Slope

Standards

- Applies to graded slopes ranging from 2:1 to 1:1
- Planter areas between walls (benches) will be between 8 and 10 feet in width to promote a natural appearance. Extraordinary circumstances will allow for planter areas between walls to be less than 8 feet wide with approval by Community Development Staff. In no instance may planters (benches) be less than 6 feet.
- All rockery walls shall be treated with permeon or equivalent where they differ from the color of the surrounding topography.
- Rockery walls shall be curved; no sharp angled walls will be allowed.
- Permanent bare rock cut slopes steeper than 2:1 may be used as an alternative to terraced rockery walls if approved by a geotechnical engineer and the Community Development Staff.

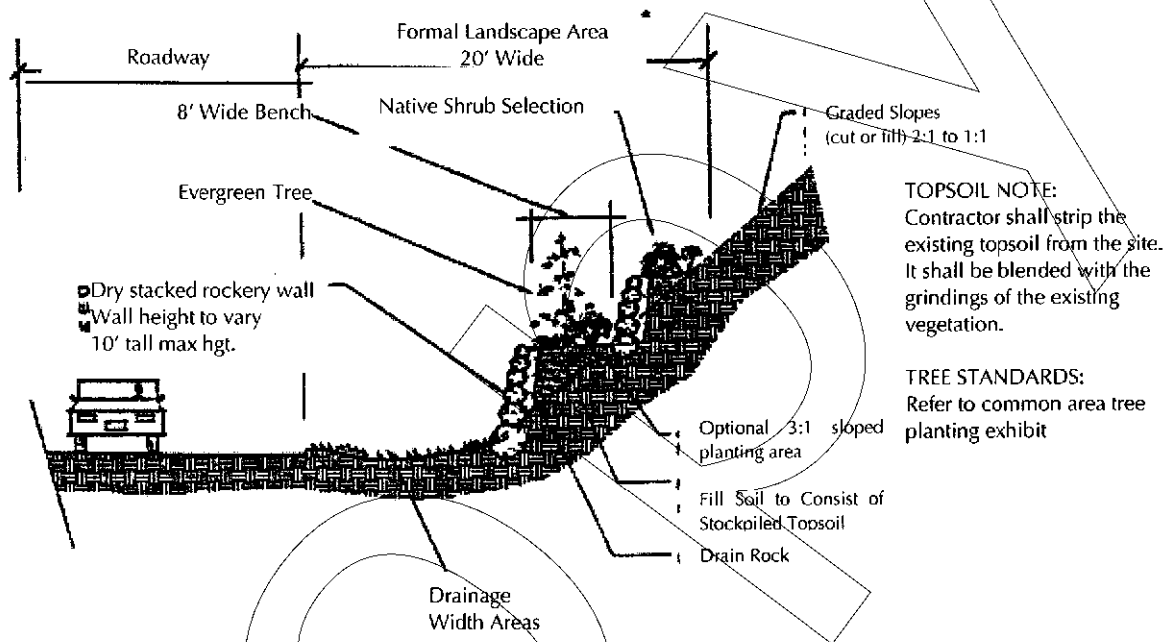


Figure 23B - Terraced Rockery Wall

Planted Top Rock Treatment (Model 2)

This concept uses large rocks that mechanically stabilize the slope creating planter pockets on the graded slope. A mix of trees and shrubs will be planted on the slope and the entire disturbed area is seeded with a native plant mix. Tree planting density of the slope may be reduced in limited areas to create a naturalistic look where appropriate. This model is dominated by evergreen plantings. The concept is especially effective in restoring the disturbed area to one that closely resembles the native conditions (refer to Table 4 – Slope Treatment Models and Figure 24A – 20% to 40% Natural Slope Range and & 24B - Planted Top Rock Treatment).

Application

This slope treatment will be required when the average natural grade for the cross slope being graded is between 20% and 40% and the proposed cross slope (cut or fill) treatment does not exceed 2:1 (refer to Table 4 – Slope Treatment Models and Figure 24A- 20% to 40% Natural Slope Range and 24B – Planted Top Rock Treatment). In addition, both horizontal and vertical variations on the slope will be used when determined appropriate by the Community Development Staff. The variations include 2:1, 3:1, 4:1, and 5:1 changes.

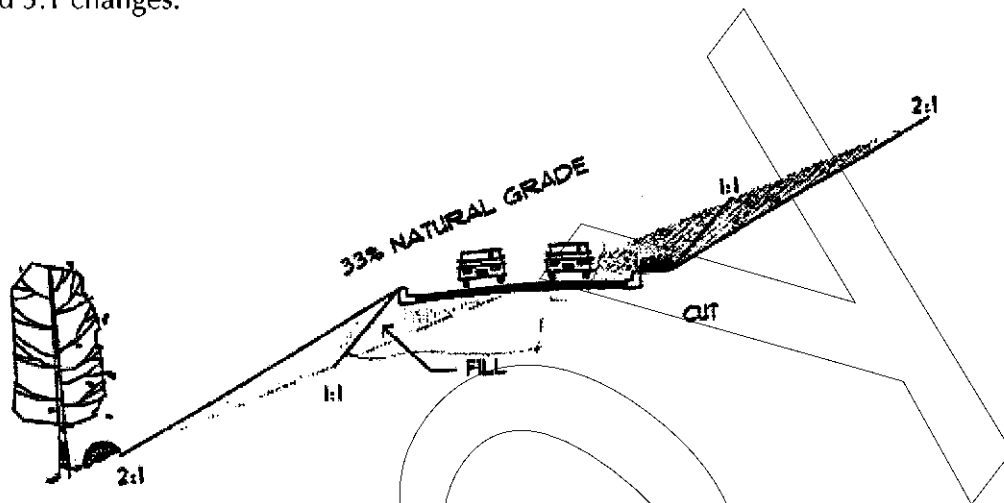


Figure 24A - 20% to 40% Natural Slope Range

Standards

- Applies to graded slopes not exceeding 2:1

TOP ROCK GRADATION AND COMPOSITION:
 10% - (12") MINUS DIAMETER
 60% - (12") TO (36") DIAMETER
 30% - (36") AND GREATER DIAMETER

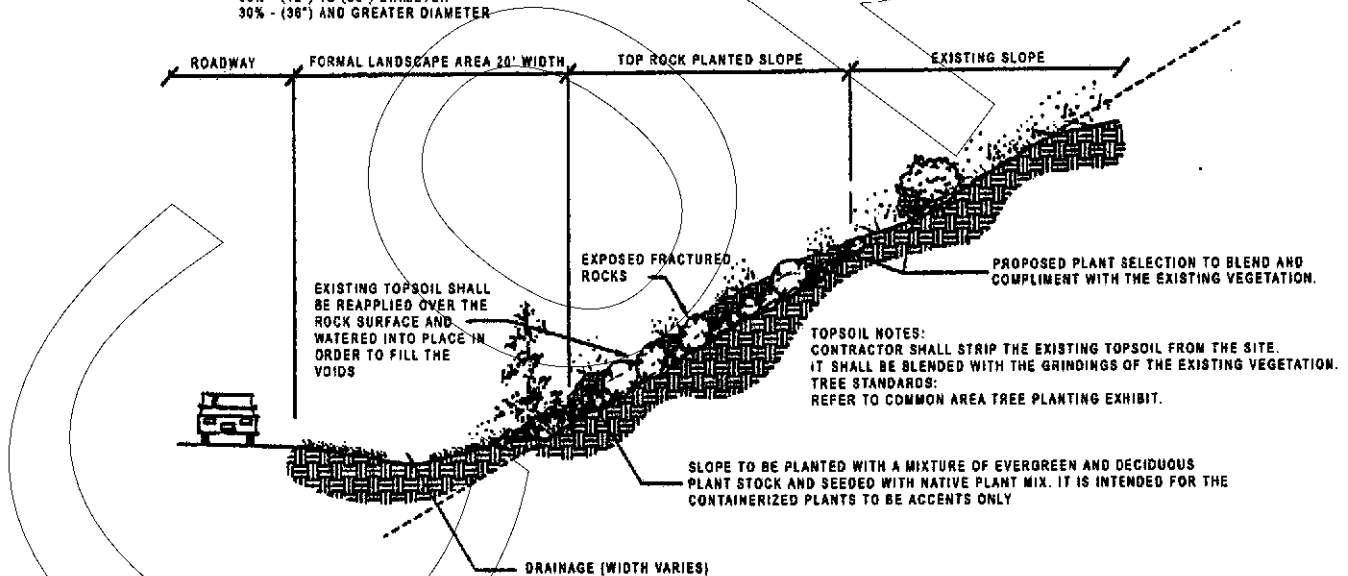


Figure 24B - Planted Top Rock Treatment

Planted Rock Talus Slope Treatment (Model 3)

This is used for the same slopes as the Model 2 "Toprock" method, but offers a variation to conform to specific site conditions. The treatment is similar to the "Top Rock" method by top-dressing and anchoring the slopes, but with a range of rock sizes (i.e. 24" minus) in combination with large boulders that create planting pockets and mechanically stabilize the slope. A mix of trees and shrubs will be planted on the slope with rock cover. This method, however, varies from the "Top Rock" method by including more exposed rock surface and less vegetative cover and is appropriate on portions of the site that are sparsely vegetated and do not include pine trees. Tree planting density of the slope is intended to create a natural look, where appropriate (refer to Figure 25A – 20% to 40% Natural Slope Range and Figure 25B- Planted Rock Talus Slope Treatment).

Application

This slope treatment will be an option when the average natural grade for the cross slope being graded is between 20% and 40% and the proposed cross slope (cut or fill) treatment does not exceed 2:1 (refer to Table 4 – Slope Treatment Type; Figure 25A – 20% to 40% Natural Slope Range and Figure 25B- Planted Rock Talus Slope Treatment). In addition, both horizontal and vertical variations on the slope will be used when determined appropriate by the Community Development Staff. The variations include 2:1, 3:1, 4:1, and 5:1 changes.

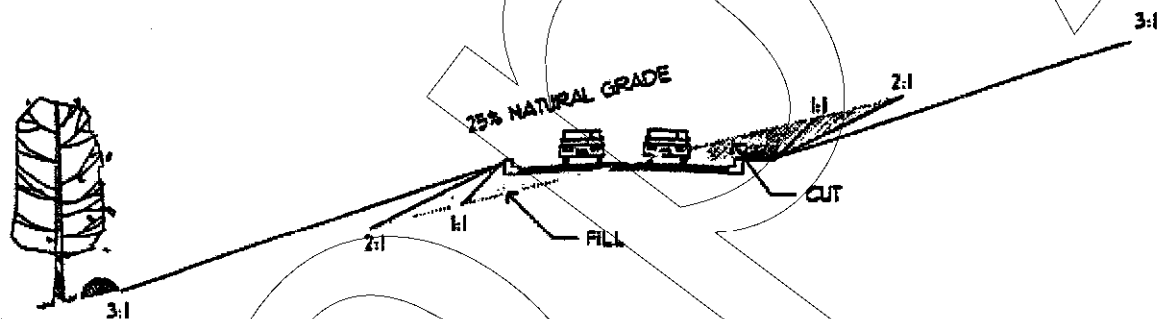


Figure 25A - 20% to 40% Natural Slope Range

Standards

- Applies to graded slopes not exceeding 2:1
- The slopes will be covered with 70% exposed rock surface and 30% vegetative cover

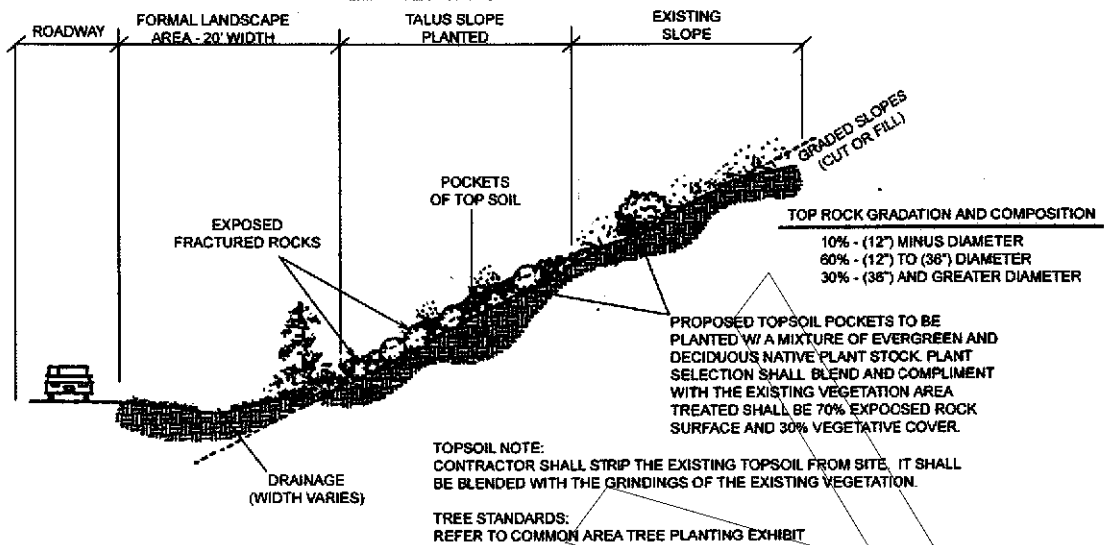


Figure 25B - Planted Talus Rock Treatment

Hydroseed Slope Detail (Model 4)

This basic concept is to apply topsoil over the graded slope, and to revegetate the disturbed area and compliment it with trees and shrub plantings, where the Community Development Staff deems appropriate. This model may be suitable where the disturbance occurs in connection with roadway construction adjacent to proposed lots. In these circumstances, treatments may need to be temporary until a home is constructed and final treatment is determined. This model may require the use of erosion control matting on the steeper slopes where geotechnical conditions identify unstable soils (refer to Figure 26B - Hydroseed Slope Treatment).

Application

This slope treatment will be an option when the average natural grade for the area being graded is between 0 and 20% and the proposed cross slope (cut or fill) treatment for the area is no greater than 3:1 (refer to Table 4 – Slope Treatment Models; Figure 26A – 20% Natural Slope and Figure 26B – Hydroseed Slope Treatment). In addition, both horizontal and vertical variations on the slope will be used when determined appropriate by the Community Development Staff. The variations include 2:1, 3:1, 4:1, and 5:1 changes.

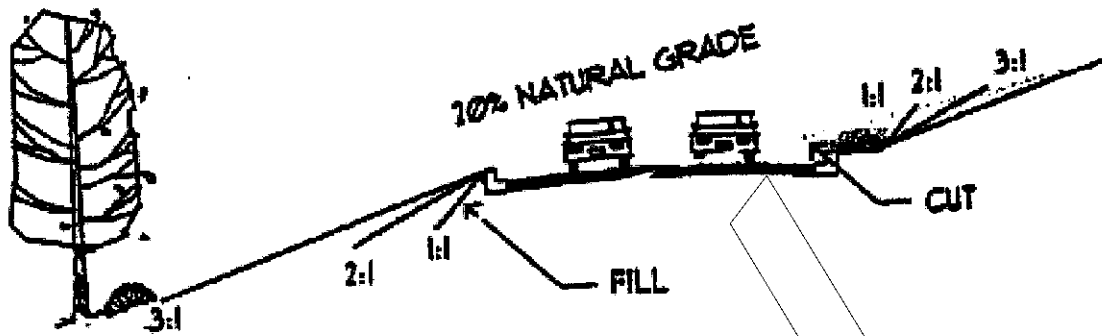
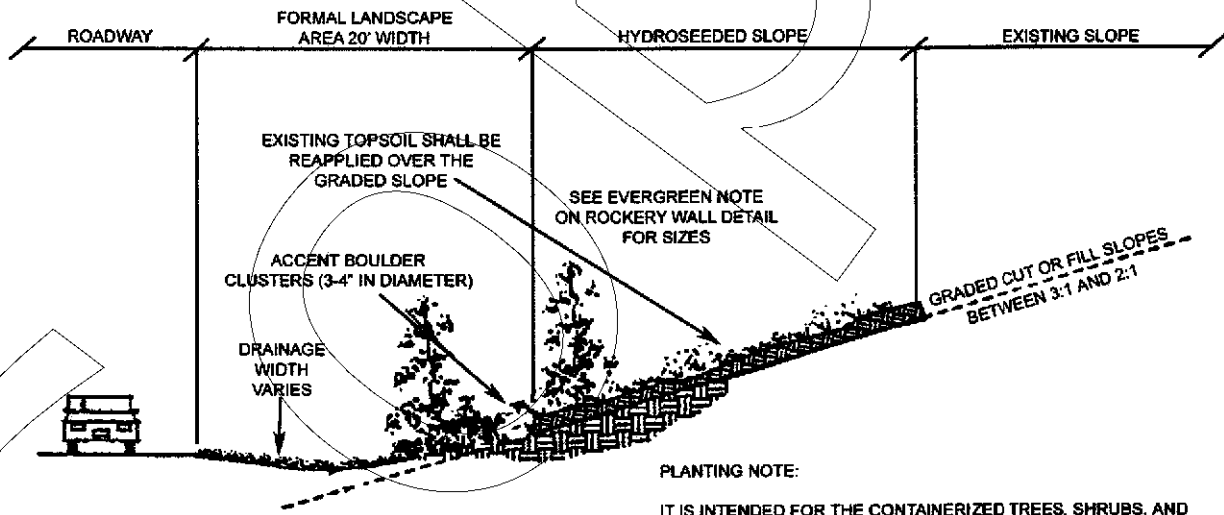


Figure 26A - 20% Natural Slope

Standards

- Applies to graded slopes that are 3:1 or less without erosion control matting or where treatments will be temporary until a home is constructed on an affected lot, or as appropriate. Otherwise, Models 2 and 3 shall prevail for 2:1 slopes.
- Revegetation mix must be approved by the Community Development Staff.
- Use of erosion control matting shall be determined by the geotechnical report with approval by the Community Development Staff.
- This treatment must otherwise comply with Chapter III of the Public Works Design Manual.



EROSION CONTROL NOTE:

EROSION CONTROL MATTING MAY BE USED IF STEEPER CONDITIONS PRESENT UNSTABLE SOIL CONDITIONS. FINAL DETERMINATION WILL BE EVALUATED WITH GEOTECHNICAL REPORT

TREE STANDARDS:

REFER TO COMMON AREA TREE PLANTING EXHIBIT

PLANTING NOTE:

IT IS INTENDED FOR THE CONTAINERIZED TREES, SHRUBS, AND BOULDERS TO BE ACCENTS ONLY. THE MAJORITY OF THE SLOPE WILL BE REVEGETATED WITH A SEED MIX THAT INCLUDES, NATIVE SHRUBS, GRASSES, AND WILDLIFE FORAGE.

TOPSOIL NOTE:

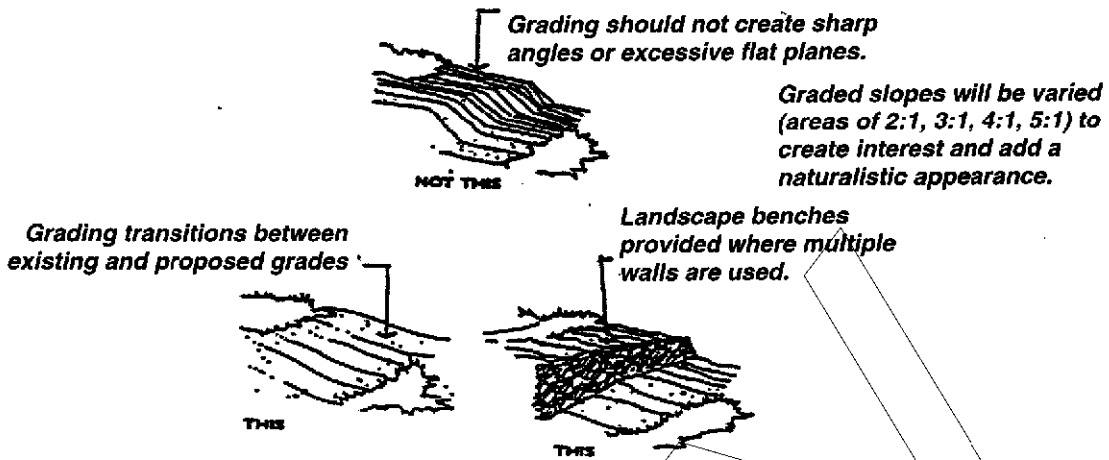
CONTRACTOR SHALL STRIP THE EXISTING TOPSOIL FROM THE SITE. IT SHALL BE BLENDED WITH THE GRINDINGS OF THE EXISTING VEGETATION

Figure 26B - Hydroseed Slope Treatment

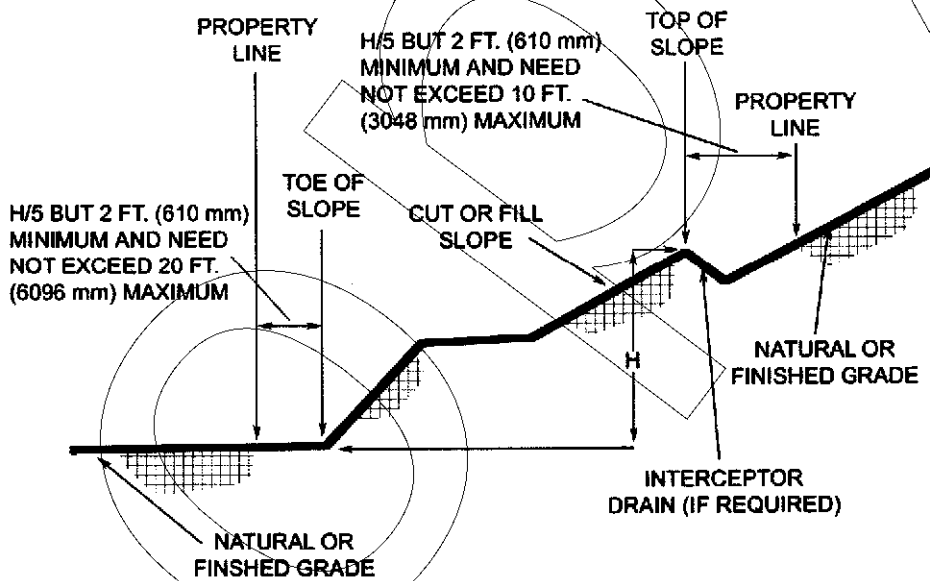
Site Grading Principles and Standards

The overall grading concept for The Pines is to create buildable lots and pad sites, where appropriate for custom and semi-custom homes and aesthetically pleasing and functional streets while maintaining the underlying integrity of the landform. Views will be considered while maintaining a low visual impact to surrounding properties. Cut and fill will be balanced on site to the extent practical.

- Where practical, export of excess fill material, if any, will be minimized by adjusting overall grades on the site and using excess material to restore severely eroded jeep trails and depressed areas created by man that currently scar the site. A special use permit shall be approved for cuts exceeding 20 feet and fills exceeding 10 feet. Cuts and fills shall be shown on the tentative map in the Appendix. If cuts and fills on any final maps exceed the limits approved by a special use permit on the tentative map, approval of a subsequent special use permit will be required.
- All disturbed areas will be revegetated or restored in accordance with Table 4 – Slope Treatment Models, and Figure 23A – 40% Natural Slope, Figure 23B – Terraced Rockery Wall, Figure 24A – 20-40% Natural Slope Range, Figure 24B – Planted Toprock Treatment, Figure 25A – 20-40% Natural Slope Range, Figure 25B – Planted Talus Rock Treatment, Figure 26A – 20% Natural Slope and Figure 26B – Hydroseed Slope Treatment. Proper erosion, dust control, and reseeding techniques will be used as required in Chapter 3 of the Public Works Design Manual. Revegetation plans shall include plans to stockpile existing topsoil and vegetative stripings and reapply the material to all disturbed areas to a minimum depth of six (6) inches. All existing dirt jeep roads, miscellaneous trails and access points not identified in Figure 16 - Trail Plan that will not be incorporated into the overall trail system will be scarified and revegetated. Revegetation standards for roads shall include the loosening of soil, placing excess fill material (if any) in disturbed areas within the limits of code application of salvaged topsoil and diverting unnatural drainage courses caused by the dirt roads and temporary irrigation (if deemed necessary by the City's Engineer and Landscape Architect).
- Seeding will be performed after finish grading has been completed, in the early spring or fall, whichever comes first. All seeded areas within developed areas will be temporarily or permanently irrigated where determined appropriate by the Community Development Staff. Seeding areas within transitional areas will, at a minimum, be irrigated for two growing seasons or until plantings are established, pursuant to 18.06 and 18.09 of the Public Works Manual.
- Transitions at top and toe of graded slopes will be rounded to blend with the natural terrain. Abrupt, squared off transitions are not permitted. In addition, both horizontal and vertical variations on the slope will be used as determined appropriate by the Community Development Staff.

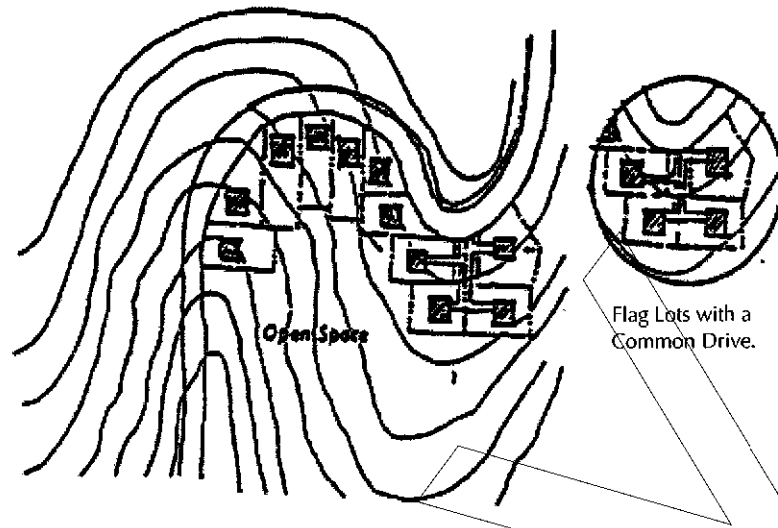


- All slope grading to include benches/terraces as shown below in accordance with Figure J109 of Appendix J of the International Building Code (IBC) with minimum slope setbacks.



- Naturalistic grading will be used where complex recontouring and revegetation must occur. Continuous expanses of land forms will be created to look natural as opposed to contrived or manmade. Where used, architectural or structured berms (i.e. retaining walls, sculptural land forms, etc.) will be an integral part of the architectural and landscape theme of the project, including consideration of color. Darker colors are preferred except where native materials are used. If necessary, permeon will be used to stain rockery walls and talus slopes to blend in with the colors of the surrounding topography.

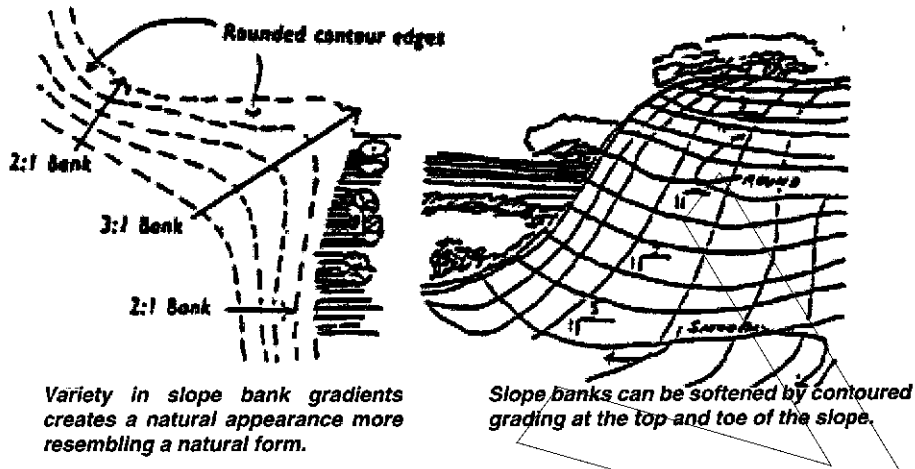
- Individual lots, when graded, shall direct runoff away from buildings and into drainage facilities.
- Construction of accessory structures is prohibited on slopes 3:1 or greater.
- The design of homes located on or adjacent to the significant ridgeline (lots 54-66 as shown on the Tentative Map in the Appendix) (as of the date of the PSDS handbook under the ownership of Fuller) shall be sensitive to views from off-site locations especially from the east. To the extent possible existing mature trees in this area shall be retained. In general, building rooflines shall be no higher than one story above the ridgeline unless creative designs, setback factors, and circumstances such as heavy tree cover creating the practical ridgeline silhouette exist or can be created and are approved by the CRACC and the Community Development Staff. House and roof colors and materials shall be neutral brown hues that blend with the existing vegetation and shall be approved by the CRACC and the Community Development Staff.
- Buildings shall be nestled into the hillside to reduce effective visual bulk and to provide energy efficient and environmentally desirable spaces. The visual area of the buildings can be minimized through a combined use of regrading and landscaping techniques. Split pads, stepped footing, pier and grade beam foundations to permit the structure to step up the slope and similar hillside architecture shall be used. Large single form structures that do not respect topography shall not be permitted. Final architecture of each home shall be reviewed and approved by the CRACC and the Community Development Staff.
- Lots containing slopes of 2:1 or greater shall be minimized. Where these conditions apply, building envelopes will be restricted and the lot owners will be required to provide creative hillside adaptive architectural design solutions. Where slopes are steep, these areas shall remain undisturbed with native landscaping.
- Flag lots and private driveways with parking adjacent to roadways shall be allowed to encourage terracing of buildings while minimizing roadway cut and fill.
- Driveways will minimize grading by following contours to the greatest extent possible.
- Where appropriate, a shared access/driveway is the preferred method for lots with slopes over 30% to minimize disturbance to hillsides. Shared driveways shall be privately owned and maintained by the HOA, the property owners subject to the shared situation, or equivalent and have adequate cross access easements and adequate "no parking" signage and demarcations per City standards. Shared driveways shall be shown on the tentative map and final map.



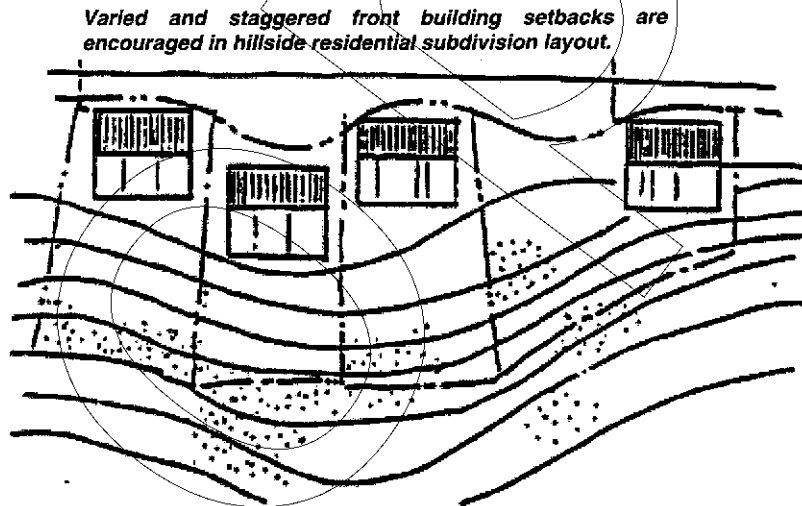
- Drainage devices such as terrace drains, benches or downdrains should be placed in locations of least visibility on slopes. The side of a drain may be bermed to conceal it. Natural swales leading downhill are a good location for downdrains. Visible concrete drains should be color tinted with permeon and screened with planting to be less obtrusive.



- New building sites should be graded such that they appear to emerge from the slope. Creation of flat areas on slopes greater than 20% shall be minimized and shall be in conformance with the approved tentative map.
- Every attempt shall be made to avoid a manufactured appearance by creating smooth flowing contours of varying gradients, preferably with slopes ranging from 2:1 to 5:1. Sharp cuts and fills and linear slopes that have uniform grade shall also be avoided.



- Varied and staggered front building setbacks a minimum of 5 feet are encouraged, where practical, in hillside residential subdivision layout. This is consistent with the natural hillside character and will reduce the monotony of repetitive setbacks.



- Building bulk may be reduced by breaking the roof form into smaller parts, reflecting the irregular forms of the surroundings. There should be a consistency of roof pitch and design

among separate roof components. Abrupt changes in eave heights will require articulation to make transitions between building components.



Not Acceptable



Acceptable

- To the extent possible, streets, driveways, parking and emergency vehicle access should be aligned to conform, as closely as possible, to the existing grades and minimize the need for the grading of slopes. They should not greatly alter the physical and visual character of the hillside by creating large notches in ridgelines or by defining wide straight alignments on hillsides. Natural land forms may often be retained by introducing gently horizontal and vertical curves in road alignment.

Landscaping Standards

Slope/Common Areas

- All rocky wall benches and landscaped slopes shall provide erosion control and 75% vegetation coverage within 2 years of planting. All disturbed slopes that will be revegetated shall be treated in accordance with Chapter 3 of the Public Works Design Manual including temporary irrigation. Disturbed slope and revegetation areas shall be shown on the tentative map and shall provide 50% vegetation coverage within 2 years of planting. This standard shall be verified by a licensed landscape architect as part of the improvement plans for all final maps.
- Trees required within the common area may be grouped to result in a natural appearance while taking into account view corridors, as well as providing appropriate screening from surrounding residences.
- All areas designated for "formal" landscaping shall provide 75% vegetative coverage within 3 years of planting, with verification provided by a licensed landscape architect to the

Community Development Staff. All shrubs and trees shall be permanently irrigated, unless otherwise approved by the Community Development Staff.

Individual Lot Landscaping

- Final landscape and irrigation plans including permanent slope treatments for each individual lot will be provided with each building permit and installed prior to issuance of certificate of occupancy by the individual homeowner and/or builder. Each lot that has direct access to a 40 foot wide street as defined in Figure 8A and 8B – 40' Street Sections shall provide at least 1 tree per 30 lineal feet of lot frontage. These trees may be deciduous in accordance with RMC. Removal of existing pine trees over 12 inches in diameter on the individual lots and specific lot landscaping plans shall be approved by the CRACC. If required by the CRACC, existing pine trees that are over 12 inches in diameter may have to be replaced at a 1:1 ratio on the lot with a 6 foot high evergreen tree. Where trees 12 inches in diameter are saved on the individual lots due to grading adjustments and lot design creativity, a 2:1 tree credit to off-set tree replacement requirements above will be applied.

The CRACC will be responsible for ensuring that individual lot owners follow all requirements applicable to lot development found in the PSDS handbook.

Fencing

It is the intent to minimize the use of fencing in The Pines to maintain the overall open and natural characteristics of the property. Split rail wood fencing is the only type of fencing that may be used along property lines if approved by the CRACC. The appropriateness of split rail fencing and whether or not it will be allowed on some lots or required on all lots within a particular development area will be determined by the CRACC with the review of the first plan for an individual residence in each development (final map) area. Other fencing may be used in the interior portions of the site for security, the safety of small children, and to control pets, however, such fencing shall be compatible with the architecture of the home and must be approved on a case by case basis by the CRACC.

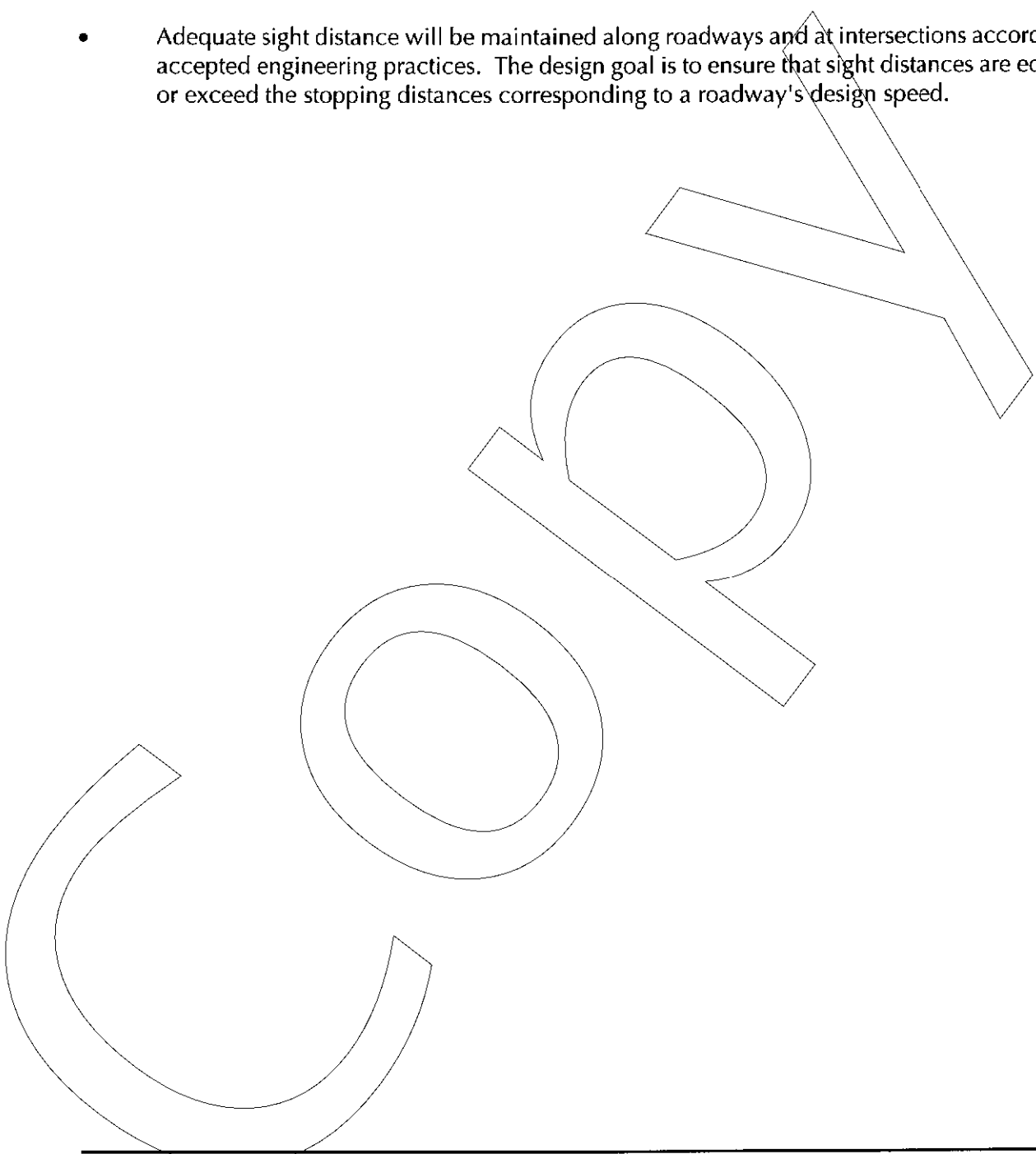
Design criteria:

- Solid lot line fencing in The Pines is prohibited. Lot line fencing may only include split rail wood fences as approved by the CRACC.
- Fencing for security, the safety of small children, where steep slopes occur and to control pets, if necessary, may be used on the interior portions of the site if compatible with the architecture of the homes and shall be approved by the CRACC.
- Privacy fences and walls for interior courtyards, swimming pools and driveway or front yard entry features may be considered on an individual house design basis as approved by the CRACC.
- The use of black, brown, or green vinyl chain-link fencing will be minimized throughout the development. Black, brown, or green vinyl chain-link may be considered on an individual

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house design basis as approved by the CRACC and where this type of fencing is required (possibly for pet enclosure areas, swimming pools or tennis courts). The color chosen must blend in with the native environment of where the fencing will be placed to the greatest extent possible.

- Split rail wood fencing in common areas including trailheads and along drainageways may be used if desired by the HOA.
- Adequate sight distance will be maintained along roadways and at intersections according to accepted engineering practices. The design goal is to ensure that sight distances are equal to or exceed the stopping distances corresponding to a roadway's design speed.



Signs

Signs will be limited to entry sign(s), street address signs, street signs, wildlife notification signs and directional signs associated with access to USFS lands and trailheads and trailhead bulletin board signs (refer to Figure 27 – Sign Plan). Sign standards presented are purposely intended not to be overly prescriptive to allow for the artistic development of signs to be concurrent with the more detailed development of the architecture.

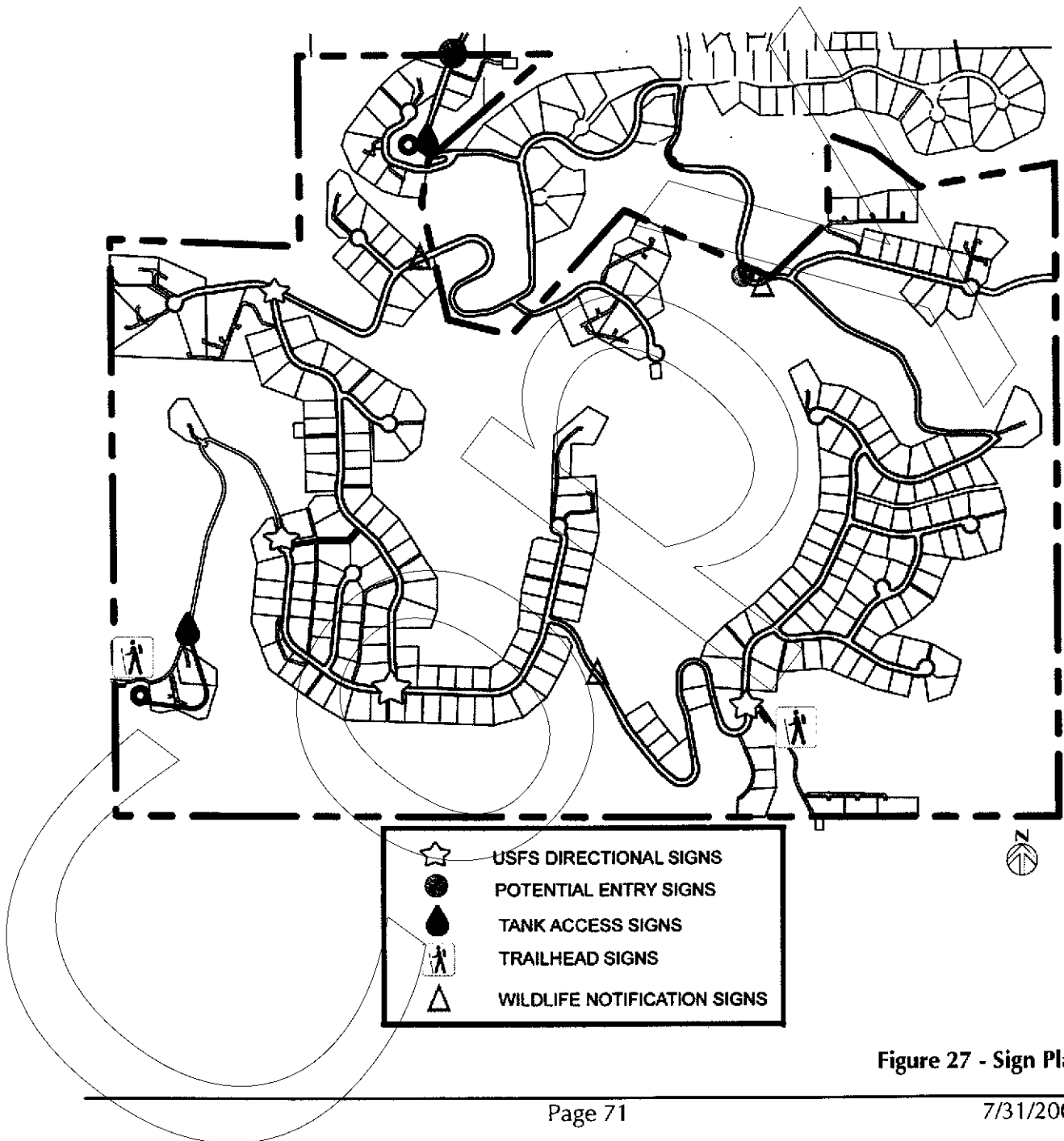


Figure 27 - Sign Plan

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- A maximum of two indirectly illuminated or non-illuminated project identification entry monument signs (14'L x 6'W x 6'H in size) shall be permitted. The entry sign(s) may be located on Pinehaven Road and on Pinebluff Trail at their entry points to The Pines (refer to Figure 28-Entry Sign Concept).
- The entry monument sign(s) will be integrated into the topography which it is located in. Sign materials will consist of either concrete, stucco, rock, wrought iron/metal or a combination of these materials consistent with the theme of the project. Enhanced landscaping shall be provided adjacent to entry monument signs as depicted in Figure 28 – Entry Sign Concept.

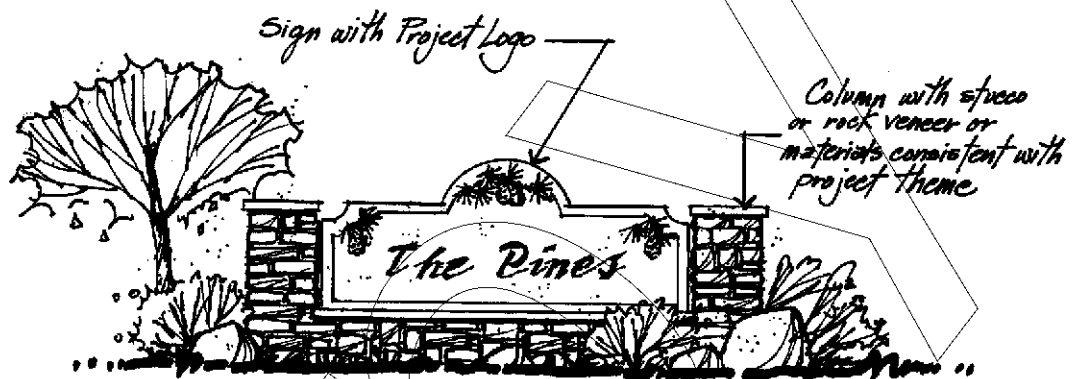


Figure 28 – Entry Sign Concept

- USFS access signs will be strategically located throughout the development to direct the public to trailhead access as shown on Figure 27, Sign Plan. In addition, trailhead signage (bulletin board format) as approved by the USFS, the Parks Department and the Community Development Staff will be provided at the two trailhead locations.
- Wildlife notification signs will be located strategically throughout the site. The purpose of these signs is to notify residents and visitors that they are entering an area that contains wildlife.
- Additional signs that restrict public access may be provided to identify TMWA tank access roads (refer to Figure 27 – Sign Plan).

Street Lighting

The Pines street lighting will be used to enhance safety and function while promoting aesthetics. All street lights shall be consistent and compatible throughout the project. The lights will comply with City street lighting standards and Sierra Pacific Power Company standards. Street lighting may include either standard City of Reno lights or decorative street lights at the sole discretion of the master developer that meet dark sky provisions. If decorative street lights are selected, they shall be placed throughout the entire community. All optional decorative street lights shall be identified in the processing of the first final map and shall be approved by SPPCo, and Community Development Staff.

House lighting shall be integrated with the architectural design of the individual residences. Exterior lighting fixtures mounted on the homes shall be no higher than the line of the first story eave or, where no eave exists, no higher than 20 feet above finished grade. Lights shall be shielded to prevent light spillage onto adjacent properties or streets. Flood lights are not permitted. Motion detector actuators, however, are permitted.

Open Space/Common Area Maintenance

The common areas, entry features, landscaping, walls, vegetative slopes, common area rockery walls, drainageways, and detention ponds will be maintained by the HOA. Appropriate maintenance will be regularly scheduled to ensure that the quality image of the project is protected and enhanced. Upon recordation of each final map, the property and associated common area will be annexed into the Caughlin Ranch CC&R's and shall become a part of the HOA. All of the common open space will ultimately become under ownership and maintenance of the HOA.

Minor Plan Amendment Process

Administrative waivers that are less than a 10% deviation to The Pines PSDS handbook and the Caughlin Ranch PUD may be granted by the City of Reno Zoning Administrator, when in the opinion of the Administrator, the waiver does not impact the health, safety, and welfare of the general public, that site circumstances or site topography would grant undue hardship to the applicant if the waiver is not granted, that the intent of the PSDS handbook is still met with approval of the waiver, and that granting of the waiver does not violate City codes and ordinances. Any standard may be varied up to 10% from that noted in the PSDS handbook. Any amendment that affects engineering standards must be recommended for approval by the City Engineering staff.

Conflicts

The project shall be subject to the City of Reno Public Works Design Manual standards except where street section dimensions and cul-de-sac lengths differ as outlined in the PSDS. If there are any conflicts between the PSDS handbook, the Caughlin Ranch PUD and/or the City of Reno Municipal Code, the PSDS handbook shall govern.

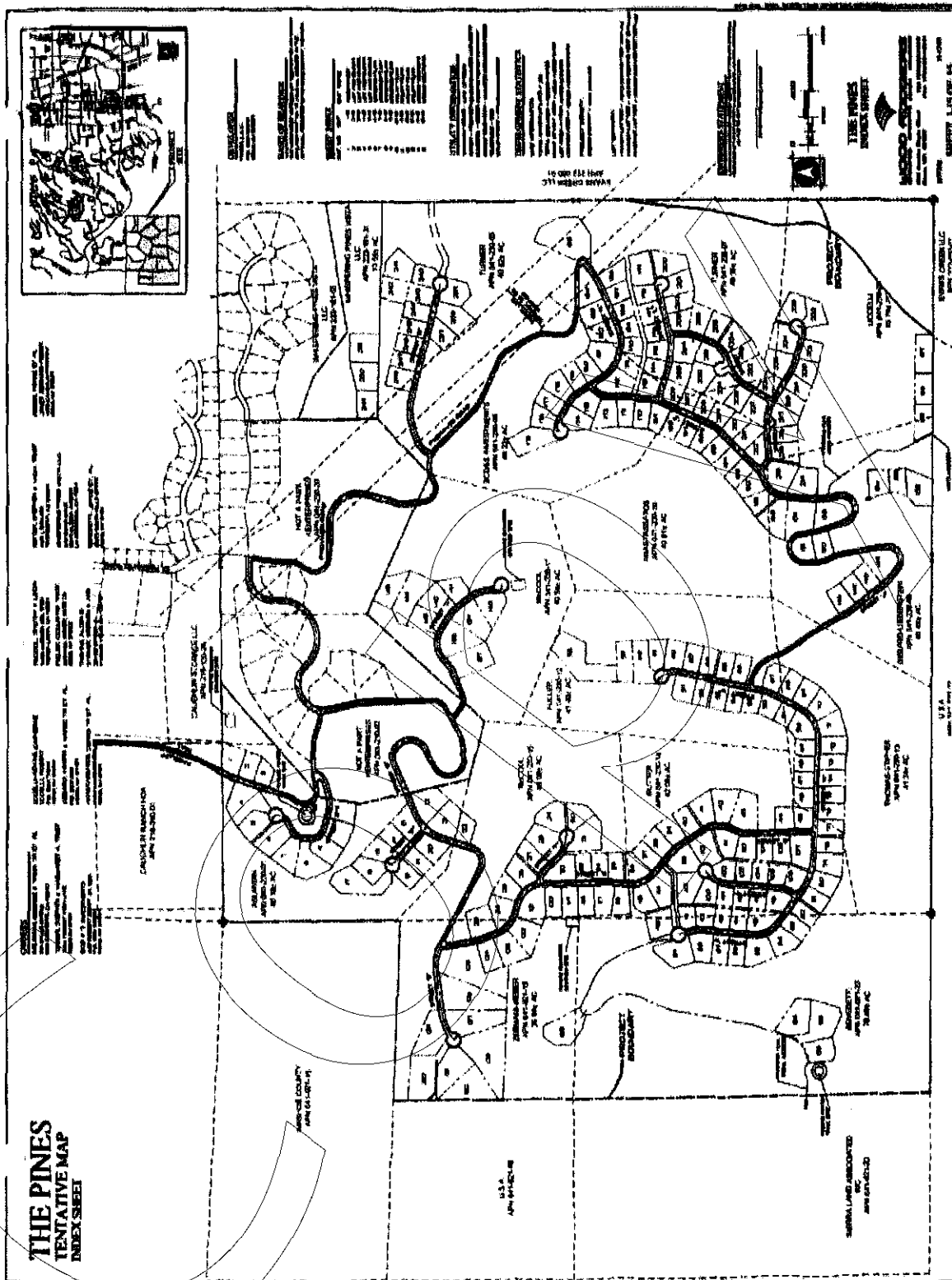


EXHIBIT A

SHEET 1 OF 15



EXHIBIT A

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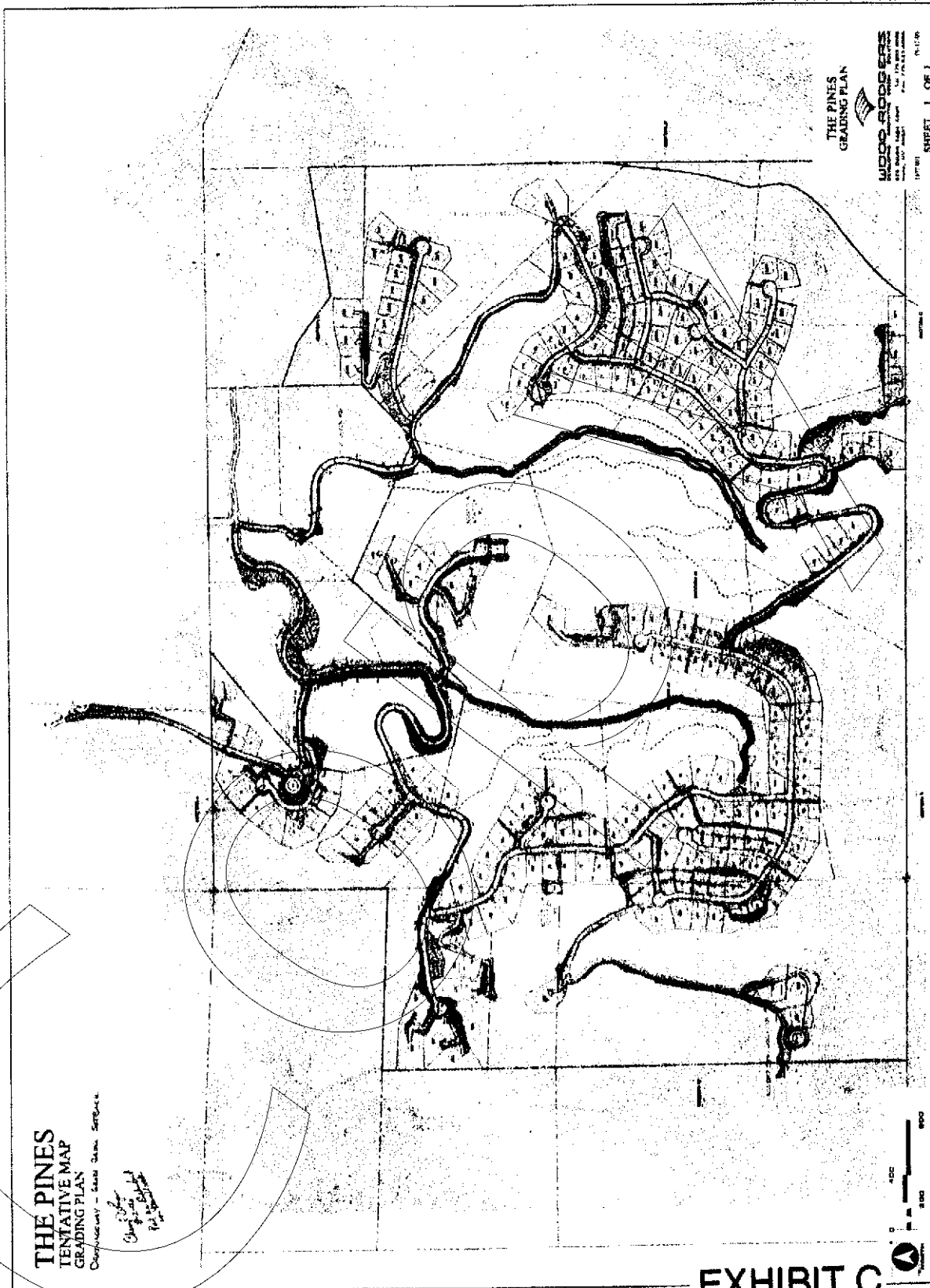
SLOPE ANALYSIS MAP
THE PINES
RENO, NEVADA
JULY, 2006



WOOD RODDERS
INCORPORATED
1000 S. 2ND ST.
RENO, NV 89502
TEL: 775-784-1111
WWW.WOODRODDERS.COM

Area	Area (Ac)	Area (Sq Ft)	Area (Sq Yd)
Area 1	1.23	84,432	9,404
Area 2	0.87	59,832	6,760
Area 3	0.45	30,960	3,484
Area 4	0.12	8,232	924
Area 5	0.05	3,468	392
Area 6	0.02	1,392	157
Area 7	0.01	696	78
Area 8	0.01	696	78
Area 9	0.01	696	78
Area 10	0.01	696	78
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EXHIBIT B 



THE PINES
 TENTATIVE MAP
 GRADING PLAN

Consulting - Land Survey Services

Widd Riddgers
 10/1/06

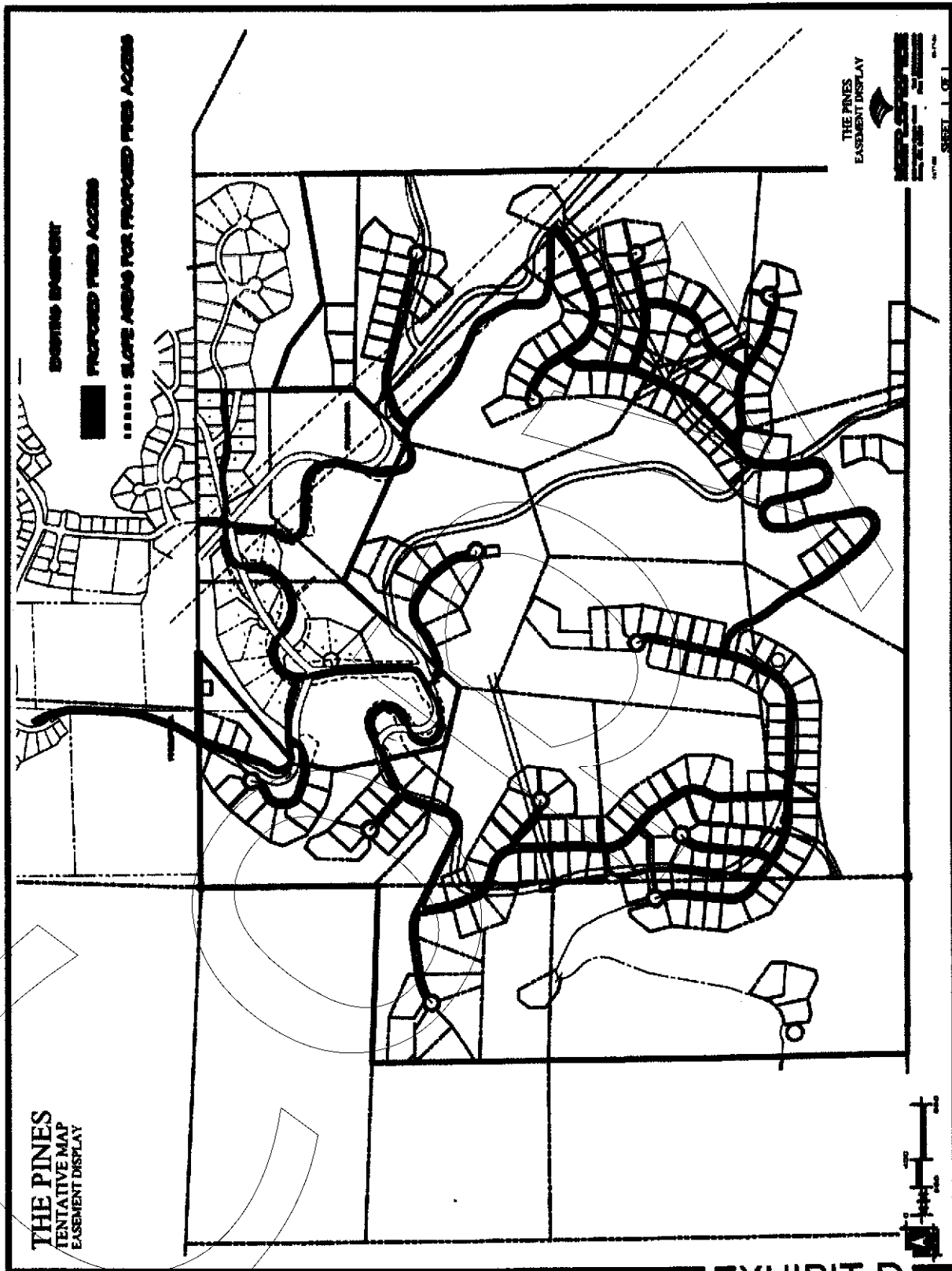
THE PINES
 GRADING PLAN



WIDD RIDDGERS
 10000 W. 10th Street, Suite 100
 Denver, CO 80231
 Phone: 303.751.4444
 Fax: 303.751.4444

10/1/06 SHEET 1 OF 1
 WIDD RIDDGERS

EXHIBIT C



THE PINES
TENTATIVE MAP
EASEMENT DISPLAY

THE PINES
EASEMENT DISPLAY

SHEET 1 OF 1

EXHIBIT D

Landscape Planting Palettes

The following lists provide opportunity for creative landscape design within the parameters of the overall project design theme.

Plant Palette

The plant palette shall be used in common open space areas and along the streets. Other plant materials may be allowed on a case by case basis per CRACC and the Community Development Staff.

LARGE SHADE TREES

Botanical/Latin Name

Acer rubrum
Betula pendula
Carpinus betulus
Eleagnus angustifolia
Fraxinus excelsior spp.

Plantanus acerifolia

Quercus robur spp.
Tilia cordata
Gleditsia triacanthos inermis
Robinia pseudoacacia 'Purple Robe'

Common Name

Scarlet Maple
European White Birch
European Hornbeam
Russian Olive
European Ash,
Weeping European Ash
London Plane Tree
(Not Suitable for Street Tree Planting)
English or Fastigate English Oak
Little-leaf Linden
Thornless Honeylocust
Purple Robe Locust

EVERGREEN TREES

Botanical/Latin Name

Abies concolor
Thuja occidentalis
Buxus microphylla
Picea abies
Pinus sylvestris
Cypressus sp.
Juniperus scopulorum
Juniperus v. "Skyrocket"
Pinus edulis

Common Name

White Fir
American Arborvitae
Japanese Box 'Winter Gem'
Norway Spruce
Scotch Pine
Cypress species
Rocky Mountain Juniper
Skyrocket Juniper
Pinyon Pine

MEDIUM-SMALL DECIDUOUS TREES

Botanical/Latin Name

Acer ginnala
Koelreuteria paniculata

Common Name

Amur Maple
Golden Rain Tree

ORCHARD TREES & PLANTSBotanical/Latin Name

Chaenomeles spp.
 Malus spp.
 Prunus spp.
 Prunus spp.
 Prunus spp.
 Pyrus calleryana
 'Bradford', or 'Redspire'
 Sorbus Aucuparia

Common Name

Quince
 Apple
 Flowering Almond
 Apricot
 Plum
 Pear
 European Mountain Ash

EVERGREEN SHRUBSBotanical/Latin Name

Juniperus sp.
 Hex crenata
 Picea abies 'sherwoodii'
 Pinus mugo mugo
 Thuja spp.
 Arctostaphylos patula
 Artemisia tridentate
 Cercocarpus
 Chrysothamnus nauseosus
 Cytissus sp.
 Ephedra viridis
 Yucca sp.

Common Name

Juniper
 Japanese Holly
 Miniature Norway Spruce
 Dwarf Mugho Pine
 Arborvitae
 Greenleaf Manzanita
 Big Sage
 Mountain Mahogany
 Rabbit Brush
 Broom
 Mormon Tea
 Yucca

DECIDUOUS SHRUBSBotanical/Latin Name

Acer campestre
 Buddleia davidii
 Cornus sanguinea
 Berberis thunbergii
 Euonymus alatus
 Forsythia x intermedia
 Hibiscus syriacus
 Potentilla sp.
 Ribes sp.
 Rosa sp.
 Syringa vulgaris
 Spiraea sp.
 Artemisia schmidtiana
 Cotoneaster sp.
 Cowania Mexicana
 Fallugia paradoxa
 Perovskia atriplicifolia
 Prunus besseyi
 Purshia tridentata

Common Name

Hedge Maple
 Butterfly bush
 Bloodtwig Dogwood
 Japanese Barberry
 Winged Eunoymus
 Forsythia
 Rose of Sharon
 Cinquefoil
 Alpine or Golden Currant
 Old Rose (**not** teas)
 Common Lilac
 Spiraea
 Silver Mound
 Cotoneaster
 Clifrose
 Apache Plume
 Russian Sage
 Sand Cherry
 Bitterbrush

GROUNDCOVERS/VINESBotanical/Latin Name

Achillia tomentosa
 Ajuga reptans
 Clematis sp.
 Euonymus fortunei
 Hedera helix
 Juniperus sp.
 Lonicera japonica
 Nepeta faassenii
 Parthenocissus quinquefolia
 Rosa 'Altissimo'
 'Blaze', or 'Don Juan'
 Sedum sp.
 Vinca minor
 Vitus 'Catawba', 'Glenora',
 or 'Verdelet' (spp. That do best in Reno)
 Wisteria sinensis or floribunda
 Arctostaphylos uva ursi
 Phlox subulata
 Santolina chamaecyparissus
 Santolina virens

Common Name

Woolly Yarrow
 Carpet Bugle
 Clematis (Vine)
 Winter Creeper
 English Ivy (Vine)
 Juniper
 Honeysuckle
 Catmint
 Virginia Creeper (Vine)
 Climbing Rose (Vine)

 Sedum
 Dwarf Periwinkle

 Grape
 Wisteria (Vine)
 Kinnikinnick
 Creeping Phlox
 Lavender Cotton
 Green Lavender Cotton

PERENNIALS

Any perennials that are hardy to Sunset Zone 3 are approved for use in the Developed palette.

TURF GRASSBotanical/Latin Name

Festuca arundinacae
 Poa pratensis
 Elymus cohereus
 Elymus glaucous
 Festuca ovina Glauca'
 Helictotrichon sempervirens
 Miscanthus sinensis 'Gracillimus'
 Oryzopsis hymenoides
 Poa nevadensis
 Pennisetum setaceum
 Stipa gigantean

Common Name

Tall Fescue
 Kentucky Bluegrass
 Basin Wildrye
 Blue Wild Rye
 Blue Fescue
 Blue Oat Grass
 Maiden Grass
 Indian Ricegrass

 Fountain Grass
 Giant Feather Grass



KENNY C. GUINN
Governor

SCOTT K. SISCO
Interim Director

STATE OF NEVADA
DEPARTMENT OF CULTURAL AFFAIRS

Nevada State Historic Preservation Office

100 N. Stewart Street

Carson City, Nevada 89701

(775) 684-3448 • Fax (775) 684-3442

www.nvshpo.org

RONALD M. JAMES
State Historic Preservation Officer

March 14, 2006

Cheryl Ryan
Senior Planner
City of Reno
P.O. Box 1900
Reno NV 89505

RE: The Pines, Carson Range, Washoe County (LDC06-00376).

Dear Ms. Ryan:

The Nevada State Historic Preservation Office (SHPO) reviewed the Community Development Department's memorandum of February 28, 2006 and the proposed project. In order to determine the archaeological sensitivity of the project area, the SHPO consulted the online statewide archaeological inventory (NVCRIS). According to these records, the project area has not been formally inventoried for cultural resources. However, informal surveys by professional archaeologists immediately west of Section 33, Township 19 North, Range 19 East have revealed significant prehistoric archaeological resources that, although not evaluated, would very likely be considered regionally significant. As a result, the SHPO strongly recommends an archaeological inventory of the proposed development.

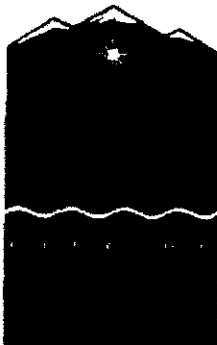
Please note that the SHPO did not review this project for agency compliance with Section 106 of the National Historic Preservation Act of 1966, as amended. If federal funds or a federal permit are required for the completion of this project, federal agency consultation with this office is necessary for compliance with Section 106 of the National Historic Preservation Act of 1966, as amended.

If you have any questions concerning this correspondence, please contact me by phone at (775) 684-3443 or by E-mail at rpalmer@clan.lib.nv.us.

Sincerely,

Rebecca Lynn Palmer
Review and Compliance Officer, Archaeologist

EXHIBIT F



Lynnette R. Jones
City Clerk
(775) 334-2030
ljones@ci.reno.nv.us

Carmi D. Gundersen
Chief Deputy City Clerk
(775) 334-2030
gundersen@ci.reno.nv.us

Office of the City Clerk
Central Callroom (775)334-2032
Parking Tickets (775)334-2279

Steven D. Whitaker, CRM
Records Systems Manager
(775) 326-6633

RECEIVED WR1-RENO

MAR 12 2007

March 8, 2007

Melissa Lindell
Wood Rodgers
575 Double Eagle Court
Reno, NV 89521

FILED THIS DATE
3 19 07
BY: [Signature]
CITY CLERK

RE: Case No. LDC06-00376 (The Pines)

Dear Applicant:

At a regular meeting held February 28, 2007, the Council upheld the Planning Commission recommendation and approved the request for the following:

- A. Annexation of ±120.28 acres, by ordinance;
- B. A zoning map amendment to (a) expand the "The Pines" PUD (Planned Unit Development) boundaries as shown in "The Pines PUD Supplemental Handbook"; (b) amend the Caughlin Ranch PUD Handbook to add the ±160.78 acres of land and insert associated text, making the property subject to the development standards and policies of the Caughlin Ranch PUD Handbook; and (c) to change the zoning designation on ±120.28 acres from HDR-2.5 (High Density Rural - 2.5 acre lots) to PUD and ±40.53 acres from SPD (Specific Plan District) to PUD, by ordinance;
- C. A tentative map for 251 single family units, subject to the following conditions; and

EXHIBIT G.



Lynnette R. Jones
City Clerk
(775) 334-2030
ljones@ci.reno.nv.us

Carmi D. Gundersen
Chief Deputy City Clerk
(775) 334-2030
gundersen@ci.reno.nv.us

Office of the City Clerk
Central Casework (775)334-2032
Parking Tickets (775)334-2279

Steven D. Whitaker, CRM
Records Systems Manager
(775) 326-6633

RECEIVED WARRIOR

APR 03 2007

March 30, 2007

Melissa Lindell
Wood Rodgers
575 Double Eagle Court
Reno, NV 89521

FILED THIS DATE

3130107

BY: *[Signature]*
CITY CLERK

RE: Case No. LDC06-00376 (The Pines) - **NOTICE OF FINAL ACTION, DECISION OR ORDER**

Dear Applicant:

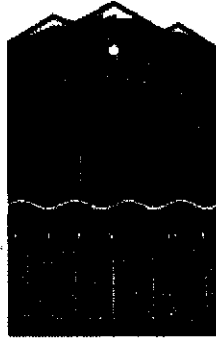
At a regular meeting held March 28, 2007, the City Council passed and adopted Ordinance No. 5912, approving the annexation and Ordinance No. 5913, approving the zoning map amendment for the above case, respectively.

Sincerely,

Carmi Gundersen for

Lynnette R. Jones
City Clerk

LRJ:edg



Lynnette R. Jones
City Clerk
(775) 334-2030
ljones@ci.reno.nv.us

Carmi D. Gundersen
Chief Deputy City Clerk
(775) 334-2030
gundersen@ci.reno.nv.us

Office of the City Clerk
Central Cashiering (775)334-2032
Parking Tickets (775)334-2279

Steven D. Whitaker, CRM
Records Systems Manager
(775) 326-6633

July 12, 2007

FILED THIS DATE
7/12/07
BY: [Signature]
CITY CLERK

Melissa Lindell
Wood Rodgers
575 Double Eagle Court
Reno, NV 89521

RE: Case No. LDC06-00376 (The Pines) – Request for certification of The Pines Supplemental Development Standards (PSDS) Handbook (The Pines)

Dear Applicant:

At a regular meeting held June 27, 2007, the City Council certified the final plan for The Pines Final Supplemental Development Standards Handbook, which was tentatively approved by City Council on February 28, 2007, for the +625 acre property located west of South McCarran Boulevard, and more specifically located to the south, west, and east of the "Quail Valley in The Pines" and "Whispering Pines" subdivisions with access from Pinchaven Drive and Pine Bluff Drive.

In order to effectuate the final plan, it must be recorded at the Washoe County Recorder's Office in accordance with NRS 278A.

Sincerely,

Carmi Gundersen for

Lynnette R. Jones
City Clerk

LRJ:cdg

Melissa Lindell, Wood Rodgers
Case No. LDC06-00376 (The Pines) – Certification of Final Plan
July 12, 2007
Page 2

xc: Community Development
Traffic Design Engineer
Nanette Smejkal, Parks, Recreation & Community Services
Patrice Echola, Regional Transportation Commission
Chief Paul Wagner, Reno Fire Department
Reno Fire Marshal
Donald Naquin, City Landscape Architect
U. S. Forest Service
Nevada Division of Wildlife
Army Corps of Engineers
Nevada State Historic Preservation Office
Nevada Department of Transportation
George & Tania Aslanian
Stephen & Margaret A. Turner
BCD&S Investments
Robert Uccelli et al
Martin & Maree Debard
Dennis Anastassatos
Timothy & Linda McCool
Collin Fuller MD
Alton B. Thomas
Stephen & Ann Stipher
Stephen & Vicki Rutter
Boyd Cole, Whispering Pines Vista LLC
Joyce Benedetti et al
Franz Weber et al
Janet Zermani-Weber
Lory Kohlmoos, Kenterprises DBA
Stephen Mollath, Esquire

THE PINES WILDLIFE MITIGATION PLAN



EXHIBIT 1

Introduction

The purpose of The Pines Wildlife Mitigation Plan is to address wildlife habitat and the concerns and issues associated with the development of 251 home sites on the 625 acre Pines project site. This plan is incorporated into The Pines Supplemental Design Standards Handbook (PSDS) and includes examination of the open space, design of the drainageway crossings, discussions regarding treatment of disturbed common open space areas and the provision of appropriate wildlife forage seed mix in open space revegetation areas, fencing and signage as related to the wildlife habitat. The plan also outlines action measures and is in accordance with Condition 13 for Case No. LDC06-00376 (The Pines) which reads as follows:

Condition 13 – A wildlife mitigation plan approved by the Nevada Division of Wildlife shall be incorporated into the PSDS handbook prior to recordation that includes examination of the open space, design of the drainageway crossings, an appropriate wildlife forage seed mix, fencing and trail signage requiring the leashing of pets and alerting users to wildlife.

I. Property Location

The Pines development is generally located at the south end of Caughlin Ranch in southwest Reno. Adjacent land uses to the north include an existing mini storage/RV storage facility, the Traditions and Whispering Pines single family subdivisions of Caughlin Ranch, the Quail Valley in The Pines subdivision which is not a part of Caughlin Ranch and undeveloped property under the ownership of Washoe County. United States Forest Service (USFS) land is located to the west and south of the site. Undeveloped private property owned (as of the date of the PSDS handbook) by Sierra Lands Associated, Inc. is located adjacent to the southwest boundary. The Ballardini Ranch, which is also undeveloped, is located to the east and (as of the date of the PSDS handbook) is under the private ownership of Evans Creek LLC.

The site and surrounding areas include habitat for wildlife such as mule deer, bear, bobcat, mountain lions, skunks, coyotes, squirrels, snakes, quail, hawks and other birds.

II. Project Concept/Description

The Pines is a master planned development that includes a total of 251 lots. The site consists of 625+/- acres and includes two major drainageways that originate on the property. The site also includes steep and hilly terrain. Vegetation includes mature pine trees, mahogany, sagebrush, bitterbrush, grasses and other native species. Over the years, a number of roads and gathering places have been created on the site as a result of off road vehicle activity and abuse. With development, the jeep roads and gathering spots will be scarified and revegetated with native vegetation that shall include wildlife forage seed mix.

Lot sizes within the project will vary but will be a minimum of .40 acres in size. The overall density of the project is approximately .40 dwelling units per acre. To ensure the project is unique and preserves natural features and wildlife habitat, approximately 67% of the site (421.85 acres) will be retained in common open space (refer to Exhibit 1 – Overall Site Plan).

III. Review of Project Design

A. Examination of the Open Space

The Pines master plan requires at least 421.85 acres of the 625 acre site to remain in permanent open space. This constitutes 67% of the total site area. Adjacent land to the west and south that is under the ownership of the U.S. Forest Service is also anticipated to remain in open space. The project will include two trailheads with parking and signage at the edges of the property (refer to Exhibit 1 – Overall Site Plan). The open space identified within The Pines development contributes to the overall open space network for wildlife and recreational use in the general area.

In terms of this project site, the location of the planned open space is critical with regard to the preservation of wildlife habitat. Corridors along the two major drainageways will remain undeveloped and to the greatest extent natural vegetation will remain undisturbed. Corridor widths along Drainageway “A” between the proposed lots range from approximately 500 feet to 850 feet while widths along Drainageway “B” range from about 1,100 to 1,300 feet. In addition, substantial open space has been planned along the edges of the project site and steep areas will not be developed.

In summary, the large amounts of open space and the preservation of the drainageway corridors will allow wildlife the opportunity to continue to travel throughout the site.

B. Design of the Drainageway Crossings

Drainageways located on the site for the most part will remain undisturbed and left natural with the existing habitat remaining. There are two major drainageways that originate within the boundaries of the site. The site has been strategically designed so that only three roadway crossings are needed to access lots in the community. The crossings are located in the lower portions of the drainageways which are closest to the existing developed portions of Caughlin Ranch and McCarran Boulevard and the farthest away from existing undisturbed wildlife habitat.

The Pinebluff crossing of Drainageway “A” is very minor and only serves to access nine lots. Drainage flows are very low in this location and will be conveyed via a drainage culvert pipe. Traffic generation at this location will be very low. Due to the low traffic generation at this location, conflicts between vehicles and wildlife should be minimal.

The two Pinehaven crossings of Drainageway “A” and “B” will access approximately half of the project site (about 125 lots). These drainageway crossings have been designed

with oversized 4'x 6' box culverts to allow some species of wildlife to move freely under the roadway within the drainageways (refer to Figure 13A – Pinehaven Major Drainageway Crossing – Typical Section in the PSDS).

Where any drainageways are disturbed as a result of roadway crossings, the areas of disturbance will be revegetated with native vegetation and ornamental landscaping as required by the City of Reno. Landscape and revegetation plans shall include wildlife forage seed mix to the satisfaction of the City of Reno.

In summary, the drainageways (which originate on the project site) will remain undisturbed with the exemption of three roadway crossing locations at the lower reaches of the drainages. The oversizing of the culverts at the two major drainageway crossings and the revegetation efforts will reduce impacts to wildlife at the roadway crossing locations.

C. Treatment of Disturbed Open Space Areas

The project site is currently undeveloped but has been subject to off road vehicle abuse for years. With development, jeep and motorized dirt bike trails and gathering places that are currently devoid of vegetation and eroded will be scarified and revegetated with native vegetation that will include wildlife forage seed mix. Other disturbed portions of the site that will be included as open space will either be landscaped or revegetated as appropriate. Landscaping and revegetation plans will include specific wildlife forage seed mixes and will be subject to the review and approval by the City of Reno Department of Community Development.

Other significant portions of the site will remain undeveloped in its natural state. Therefore, open space wildlife habitat will either remain in its existing condition or will be enhanced with revegetation efforts per the City of Reno Department of Community Development requirements.

D. Fencing

Fencing throughout The Pines community will be minimized to ensure that an open feeling remains throughout the development. Security and privacy fencing may be considered within individual lots but only if approved by the Caughlin Ranch Architectural Committee. Solid fencing of property lines will not be permitted. This will enable wildlife to have relatively unrestricted access throughout the development, however, may cause wildlife conflict problems for area residents. Residents should be cautioned to watch small pets and children during dawn and dusk hours, as well as during the night. Residents should also be discouraged from planting trees, bushes and flowers that would attract wildlife.

E. Signage

Signs throughout the development will include directional signs associated with access to USFS lands and trailheads and signs notifying residents and visitors that they are entering an area that contains wildlife. Signage (bulletin board format) will also be provided at the two trailheads located within the project site. These signs will be used to post information regarding trails, access to USFS land, fire danger and wildlife. In addition to signage on the property, the Caughlin Ranch Homeowner's Association will be encouraged to periodically post similar information on its website and include articles with regard to coexisting with wildlife in their newsletters.

IV. Wildlife Mitigation Actions

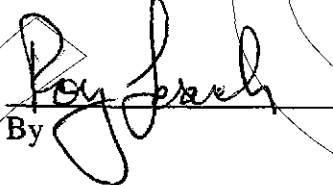
In order to mitigate impacts to wildlife as a result of the development of The Pines property, as well to help residents and visitors coexist with wildlife in this area, the following actions are required in this plan:

- 1) Maintain a minimum of 421.85 acres of open space on the project site with undisturbed drainage way corridors as shown on Exhibit 1 – Overall Site Plan.
- 2) Ensure that major drainageway crossings are minimized and are only allowed at the two locations shown in the PSDS handbook. In addition, culvert sizes and crossings of all drainageways shall be in accordance with the PSDS handbook and shall not be modified.
- 3) Redirect off road vehicles through paved roads in the development to designated trailhead areas on the property. Require existing disturbed jeep roads and gathering areas to be scarified and revegetated with native vegetation that shall include wildlife forage seed mix approved by the City of Reno Department of Community Development.
- 4) Minimize the removal of existing mature trees and native vegetation wherever possible. Require common areas that will be disturbed during development of the property to be landscaped and revegetated in accordance with the PSDS handbook with native vegetation that shall include wildlife forage seed mix approved by the City of Reno Department of Community Development.
- 5) Post signs at trailhead bulletin boards requiring the use of leashes for dogs to limit wildlife harassment and provide general information regarding coexisting with wildlife.
- 6) Encourage the Caughlin Ranch Homeowner's Association to periodically provide information on its website and in its community newsletters. and sponsor workshops with USFS and/or Nevada Division of Wildlife personnel with regard to coexisting with wildlife (at a minimum information should include requirements for leashing of dogs, discouraging landscape varieties, gardens and water sources that would attract

wildlife, containing small pets and children during dawn, dusk and nighttime hours, discouraging residents from leaving garbage cans outside during dawn, dusk and nighttime hours, providing for proper trash storage, keeping pet food indoors, etc. Encourage the Caughlin Ranch Architectural Control Committee to provide similar information to each individual lot owner when they apply for review of their house plans.

- 6) Provide at least three signs throughout the community alerting motorists to the potential presence of wildlife. These signs shall be installed along the main loop roadway to help limit wildlife and vehicle interaction and damage in accordance with the PSDS handbook.
- 3) Minimize the use of fencing to allow wildlife to move freely throughout the community. Prohibit solid fencing along lot lines in accordance with the PSDS handbook.
- 4) Mimimize street and residential lighting throughout the development to reduce wildlife disturbance at night.
- 9) Provide a general Wildlife Awareness program to the residents of the community. An awareness program for residents can help to limit wildlife and human interactions and conflicts. Such things as being bear aware and recommending that all resident use bear proof trash devices to help limit wildlife nuisance and other conflicts. Also, provide residents with awareness information on other wildlife that may be present in and around the community, such as mountain lions, bobcat, mule deer and other natural wildlife, and ways to prevent problems with wildlife (examples include such things as; not planting vegetation that would attract wildlife in to feed on, limiting sources of water that may attract wildlife, and keeping pets and pet food indoors to help limit predation by large carnivores).

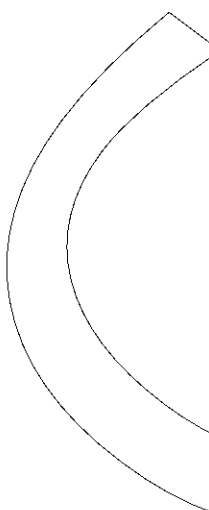
This Wildlife Mitigation Plan has been approved by the Nevada Division of Wildlife.


By _____

4-26-07
Date

THE PINES

WILDLIFE MITIGATION PLAN



	DRAINAGE
	TRAIL
	DRAINAGE CROSSING
	TRAILHEAD

Evans
Creek
LLC

USFS

Sierra
Lands
Associated

PER
REVISION



WASHOE COUNTY RECORDER

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Stacie Huggins
Signature

7/31/07
Date

Stacie Huggins
Printed Name